

HOPKINS HEIGHTS DEVELOPMENT PLAN

Coastal Hopkins River Growth Area

Warrnambool | Victoria

March 2019

MYERS
PLANNING GROUP

GROUP GSA

WARRNAMBOOL PLANNING SCHEME
Development Plan for:

Hopkins Heights

Is approved in accordance with the requirements
under Development Plan Overlay - Schedule 13.
WARRNAMBOOL CITY COUNCIL

Delegate:

Date:

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Quality Information

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1. Introduction

The Hopkins Heights Development Plan has been prepared by Myers Planning Group on behalf of Hopkins Heights Warrnambool Pty Ltd (Hopkins Heights Estate).

The Hopkins Heights Development Plan encompasses approximately 78 hectares of land and is a nominated growth area of the City of Warrnambool (Coastal Hopkins River Growth Area).

Warrnambool is an important city within the Great South Coast Region in terms of its role as a service and retail centre which supports established industries including farming and food manufacturing. Further, Warrnambool's established education presence and other supporting industries makes it a focus for employment in the region. Warrnambool is the western gateway to many of the region's tourist destinations.

Having regard to the strategic role of Warrnambool to the broader region, the purpose of the Hopkins Heights Development Plan is to guide future land use and development within the Coastal Hopkins River Growth Area.

The Hopkins Heights Development Plan was publicly exhibited in February 2019. Minor amendments and refinements were made to the Development Plan in response to submissions received and to improve the functionality of the proposed development. The Development Plan was approved by Warrnambool City Council on 1 April 2019.

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1.1 Development vision

The Hopkins Heights Development Plan vision is:

"Hopkins Heights is a thriving community with a distinctive coastal and riverine feel. Residents have the best of both worlds benefiting from proximity to the Warrnambool CBD whilst being part of a thriving community living, socialising and recreating in a holistically designed residential estate"

The lifestyle qualities of Hopkins Heights are enriched by a strong connection to the open space network which extends across the estate taking in the Hopkins River, hilltops and nearby coastal reserves. The Hopkins Heights 'village centre' and public spaces are places of greatly valued by all residents.

Hopkins Heights is a great place to live with a range of housing choices. Everything is close by and residents have the choice of walking, cycling or using public transport for their daily needs."

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1.2 Design Intent

The Hopkins Heights environs comprise a rare combination of features unique to Warrnambool, including:

- Contrasting slopes and flats;
- Proximity to the Hopkins River;
- Amazing coast, river and hinterland views;
- Large property in a single land holding.

These features provide a natural framework of opportunities and constraints from which to base a site-responsive design. This framework will naturally bring to the fore opportunities for open space and recreation experiences which strongly influence the form and location of development on the site. Distinctive character areas will arise as a result of this design orientation toward the natural elements. This design intention will provide the basis for unique community interactions by naturally linking private development around public spaces, resulting in opportunities to concentrate pedestrian activity for 'chance' and planned meetings.

The design intent for the Hopkins Heights development is to create an integrated and harmonious estate with interesting and diverse built form, high-quality vegetation, interlinked passive open spaces, accessible walking and cycling routes and ambience that encourages active and engaged community living. Hopkins Heights Estate will provide:

- A diversity of housing forms that give broad options for people regardless of age, household composition or economic, social or cultural status.
- Sufficient population to create vitality, safety and a true sense of community.
- Housing lots which respond to the topography and vistas, and an overall subdivision that is innovative and interesting with a variety of lot sizes, housing types and public spaces.
- A network of accessible pedestrian and cycling paths;
- A picturesque linear park along the elevated central ridgeline incorporating a 'village centre'.
- Pause points' within the open space network to provide people with tranquil spaces to rest quietly for chance or planned meetings while taking in impressive coast, river and hinterland views.
- A picturesque Hopkins River public reserve, incorporating car parks, recreational facilities and a shared cycling and pedestrian pathway;
- Public transport via bus route.
- Well designed, interesting, inviting, adaptable and strategically located public spaces that are safe, interconnected and available to all residents.
- Quality landscaping to create a high degree of visual amenity, excellent community spaces, vegetated backdrops to housing and soft edges to built form where seen from key viewing locations;
- Building design controls to ensure houses address key roads and streets and through siting provide great residential amenity and an interesting and diverse streetscape.
- Different character areas with carefully crafted controls to provide different housing options reflective of the attributes of the character areas.
- Careful design on steeply sloping land to best integrate housing and services with topography.

1.2 Structure of the Development Plan

The Hopkins Heights Development Plan is structured in a format which responds to the features of the area and key elements of the Coastal / Hopkins River Structure Plan. The development plan is structured as follows:

- **Planning context** which provides an overview of the Planning Policy Framework, the Municipal Planning Strategy, zones and overlays affecting the development plan area.
- **Site analysis** which explores property ownership, surrounding land uses, topography, views and vistas, natural features, existing infrastructure and access.
- **Design response** which details the proposed movement network, open space, density of development, infrastructure and staging.

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2. Site description

2.1 Site location and context

Warrnambool is Victoria's largest coastal regional city and the fastest growing population centre in southwest Victoria. To accommodate population growth, Warrnambool City Council has rezoned land within four greenfield residential growth areas (including the Coastal Hopkins River Growth Area).

The Coastal Hopkins River Growth Area is located approximately three kilometres east of the Warrnambool Central Business District. The growth area is bound by the Hopkins River to the north and the Southern Ocean to the south. There is an existing low-density residential area to the west, with rural land to the east. Primary access to the growth area is provided along Hopkins Point Road, which continues through to Allansford and the Great Ocean Road in the east.

Development within the growth area is guided by the *Coastal / Hopkins River Environment Structure Plan*, prepared by Planisphere in 2008. The growth area was rezoned in 2014 (via Warrnambool Planning Scheme Amendment C76).

The growth area is in three major ownerships (Logans Beach Coastal Village, Hopkins Ridge Estate and the subject land). Development has commenced within the growth area with a series of residential estates currently under construction.

This development plan relates to the growth area's largest land parcel (approximately 78 hectares) and is the final development plan to be prepared within the growth area. The development plan area is bound by the Hopkins River to the north, Hopkins Point Road to the south, an existing low-density residential area to the west, and developing residential land to the east (Hopkins Ridge Estate).

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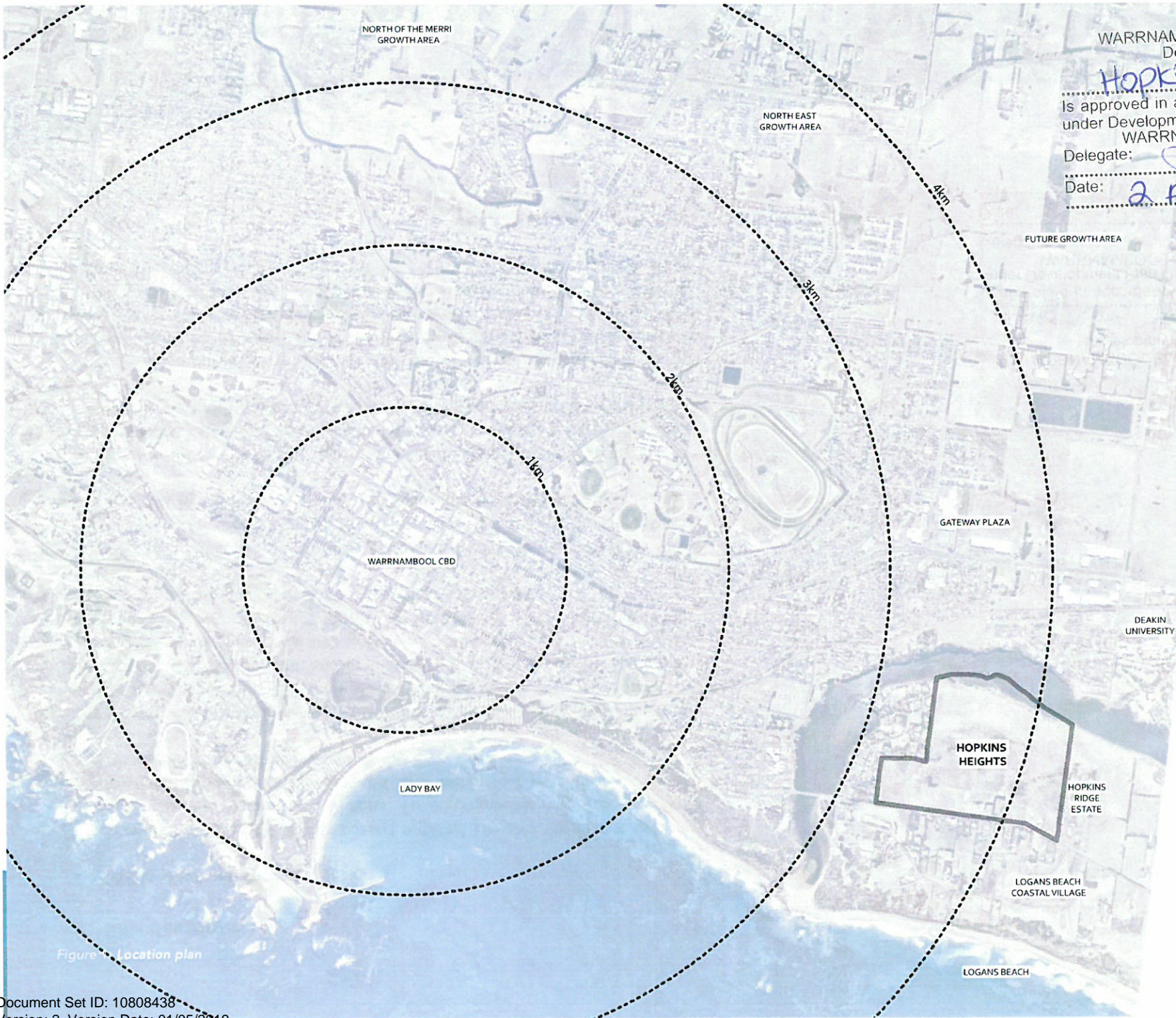
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LEGEND

Development Plan Area

Figure 1 Location plan

3. Planning context

3.1 Planning Policy Framework

The Planning Policy Framework comprises general principles for land use and development in Victoria. The following clauses of the Planning Policy Framework are relevant to this development plan:

Clause 11 'Settlement' sets out that:

Planning is to recognise the need for, and as far as practicable contribute towards:

- Health, wellbeing and safety.
- Diversity of choice.
- Adaptation in response to changing technology.
- Economic viability.
- A high standard of urban design and amenity.
- Energy efficiency.
- Prevention of pollution to land, water and air.
- Protection of environmentally sensitive areas and natural resources.
- Accessibility.
- Land use and transport integration.

Planning is to facilitate sustainable development that takes full advantage of existing settlement patterns and investment in transport, utility, social, community and commercial infrastructure and services.

Clause 11.01-1S 'Settlement' seeks to promote the sustainable growth and development of Victoria and deliver choice and opportunity for all Victorians

through a network of settlements. In addition to focusing investment and growth to places of state significance in Metropolitan Melbourne, investment and growth is also directed to major regional cities (such as Warrnambool).

Building upon this policy, Clause 11.01-1R 'Settlement - Great South Coast' supports the role of Warrnambool as the key population and employment centre within the region.

Clause 11.03-2S 'Growth areas' sets out that planning should implement the strategic directions in Growth Area Framework Plans (such as the Coastal / Hopkins River Structure Plan). Clause 11.03-2S encourages the provision of a diversity of housing types at densities which encourage efficient use of land. Clause 11.03-2S also encourages the creation of mixed-use activity centres which are high quality, well designed and create a sense of place.

Clause 12 'Environment and landscape values' set out that:

Planning should help to protect the health of ecological systems and the biodiversity they support (including ecosystems, habitats, species and genetic diversity) and conserve areas with identified environmental and landscape values.

Clause 12.02-1S 'Protection of coastal areas' encourages the protections and enhancement of coastal biodiversity and ecological values. The revegetation of cleared land abutting waterways is encouraged.

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Clause 12.03-1S 'River corridors, waterways, lakes and wetlands' seeks to protect the environmental, cultural and landscape values of water bodies. Development should be sensitively designed and sited to maintain and enhance environmental asset and landscapes along river corridors. The development plan includes a 50 metre open space corridor along the banks of the Hopkins River. Extensive revegetation is proposed within this area.

Clause 14 'Natural resource management' sets out that:

Planning is to assist in the conservation and wise use of natural resources including energy, water, land, stone and minerals to support both environmental quality and sustainable development.

Clause 14.02-1S 'Catchment planning and management' seeks to minimise the impacts of stormwater upon waterways. The development plan is accompanied with a Stormwater Management Plan (TGM Group) which sets out measures to maximise the quality of stormwater runoff from the development plan area. Stormwater will be filtered of sediment and waste prior to discharge into the Hopkins River.

Clause 15 'Built environment and heritage' set out that:

Planning is to recognise the role of urban design, building design, heritage and energy and resource efficiency in delivering liveable and sustainable cities, towns and neighbourhoods.

Planning should ensure all land use and development appropriately responds to its surrounding landscape and character, valued built form and cultural context.

Planning should protect places and sites with significant heritage, architectural, aesthetic, scientific and cultural value.

Planning must support the establishment and maintenance of communities by delivering functional, accessible, safe and diverse physical and social environments, through the appropriate location of use and development and through high quality buildings and urban design.

Planning should promote development that is environmentally sustainable and should minimize detrimental impacts on the built and natural environment.

Planning should promote excellence in the built environment and create places that:

- Are enjoyable, engaging and comfortable to be in.
- Accommodate people of all abilities, ages and cultures.
- Contribute positively to local character and sense of place.
- Reflect the particular characteristics and cultural identity of the community.
- Enhance the function, amenity and safety of the public realm.

As encouraged by Clause 15.01-1S 'Urban design', the proposed subdivision and road layout responds to natural features. Proposed landscape treatments, along with siting guidelines for future dwellings, will ensure development responds to the surrounding landscape, natural features and emerging character of the area. The public realm has been designed to be attractive with safe access to walking and cycling paths.

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Of relevance to this development plan, Clause 15.01-3S 'Subdivision design' sets out:

In the development of new residential areas and in the redevelopment of existing areas, subdivision should be designed to create liveable and sustainable communities by:

- Creating compact neighbourhoods that have walkable distances between activities.
- Developing activity centres in appropriate locations with a mix of uses and services and access to public transport.
- Creating neighbourhood centres that include services to meet day to day needs.
- Creating urban places with a strong sense of place that are functional, safe and attractive.
- Providing a range of lot sizes to suit a variety of dwelling and household types to meet the needs and aspirations of different groups of people.
- Creating landscaped streets and a network of open spaces to meet a variety of needs with links to regional parks where possible.
- Protecting and enhancing native habitat.
- Facilitating an urban structure where neighbourhoods are clustered to support larger activity centres served by high quality public transport.
- Reduce car dependency by allowing for:
 - Convenient and safe public transport.
 - Safe and attractive spaces and networks for walking and cycling.
 - Subdivision layouts that allow easy movement within and between neighbourhoods.
- A convenient and safe road network.
- Being accessible to people with disabilities.

- Creating an urban structure and providing utilities and services that enable energy efficiency, resource conservation, integrated water management and minimisation of waste and air pollution

The development plan implements this policy in the following ways:

- The development plan promotes a highly permeable movement network with a focus on safe and efficient walking and cycling paths. Residents will have the choice of walking, cycling or using public transport for their daily needs.
- A range of lot sizes are provided to suit a variety of dwelling and household types to meet the needs and aspirations of future residents.
- The development plan incorporates an open space network which extends across the estate taking in the Hopkins River and nearby coastal reserves. Landscape treatments within the open space network and streets reinforce the distinctive coastal and riverine feel of the estate.
- The development plan incorporates a conveniently located village centre to meet the day-to-day needs of the community.
- Ecological values are protected and enhanced. The development plan includes extensive revegetation along the banks of the Hopkins River and planting of native species throughout the development plan area.

Clause 15.01-4S 'Healthy neighbourhoods' seeks to ensure neighbourhoods are designed to foster community interaction and make it easy for people of all ages and abilities to live healthy lifestyles and engage in regular activity.

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The development plan has a strong emphasis on fostering community interaction and encouraging physical activity. The development plan establishes a continuous system of pedestrian and bicycle paths along all streets, linking to open space and public transport within the development plan area and beyond.

Clause 16 'Housing' sets out that:

Planning should provide for housing diversity, and ensure the efficient provision of supporting infrastructure.

Planning should ensure the long term sustainability of new housing, including access to services, walkability to activity centres, public transport, schools and open space.

Planning for housing should include the provision of land for affordable housing.

The development plan implements policy at Clause 16.01-2S 'Location of residential development' which seeks to locate new housing in designated areas which offer good access to jobs, services and transport.

Clause 16.01-3S 'Housing diversity' seeks to provide for a range of housing types to meet diverse needs. The development plan provides for a range of lot sizes to suit a variety of dwelling and household types to meet the needs and aspirations of future residents.

Clause 18 'Transport' sets out that planning should ensure an integrated and sustainable transport system. Clause 18.01-1S 'Land use and transport planning' seeks to integrate public transport services and infrastructure into new development. Clause 18.02-1S 'Sustainable personal transport' seeks to ensure

the development of new neighbourhoods provides opportunities to promote more walking and cycling. The development plan promotes a highly permeable movement network with a focus on safe and efficient walking and cycling paths. Residents will have the choice of walking, cycling or using public transport for their daily needs.

The development plan implements key policy within Clause 19 'Infrastructure' such as, Clause 19.02-6S 'Open space', Clause 19.03-2S 'Infrastructure design and provision', Clause 19.03-3S 'Water supply, sewerage and drainage', Clause 19.03-4S 'Stormwater', and Clause 19.03-5S 'Telecommunications'. The development plan encourages the provision of infrastructure in a way that is efficient, accessible and timely.

3.2 Municipal Planning Strategy

Building on the Planning Policy Framework, the Municipal Planning Strategy sets out the local and regional strategic policy context for Warrnambool. It comprises the Municipal Strategic Statement and specific local planning policies.

Clause 21.01 'Municipal Profile, Council Vision and Strategic Directions' provides an overview of the drivers for growth and change within the City of Warrnambool. The municipality includes the major centre of Warrnambool (residential, commercial and industrial land uses) and the surrounding rural hinterland. Warrnambool is Victoria's largest coastal regional city and is the main service centre for south-western Victoria.

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The city is a popular retirement and tourist destination and provides support to the region in the fields of commerce, governance, social services, health, education, the arts and recreation.

The municipality has a steadily growing population. The estimated resident population of Warrnambool is expected to increase to approximately 40,670 people by 2031. Housing supply, choice and affordability, and landscape character are key planning issues relevant to this proposal.

Clause 21.01-4 'Strategic directions' outlines residential growth is primarily directed to:

- North East Warrnambool Growth Area
- Coastal Hopkins Growth Area
- North Dennington Growth Area
- North of the Merri River Growth Area
- East of Aberline Road Future Urban Growth Corridor.

The Coastal Hopkins Growth Area is described as:

The Coastal Hopkins Growth Area immediately east and south of the Hopkins River is approximately 115 hectares of land with the potential to accommodate approximately 650 lots over the next 15 years. The growth area will provide a high quality residential environment, that responds to local characterises and context.

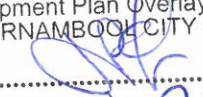
Clause 21.01-4 sets out that Council is committed to creating a sustainable city which allows people to walk or cycle to access their everyday needs and that fosters community interaction and encourages physical activity. In order to achieve this, Council has established a clear strategic direction to provide a network of activity centres and open space, and a diverse range of housing options.

Clause 21.02 'Settlement' seeks to direct urban growth to growth areas in order to protect productive rural areas and to achieve a more compact sustainable urban area. The following strategies (outlined within Clause 21.02-2 Urban Growth) are relevant to this application:

- Ensure that development is located and consolidated within settlement boundaries.
- Support increased residential densities in greenfield growth areas within close proximity to existing and/or planned transport corridors, activity centres and open space.
- Ensure new residential neighbourhoods provide a mix of housing suited to the needs of a diverse range of household types.

Clause 21.02-1 'Activity centres' encourages the establishment of 'Convenience nodes' which primarily service a small or specialised catchment

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Clause 21.03-2 'Significant environments and landscapes' sets out:

River and estuarine environments

The Hopkins estuary is one of only three 'Premier Rivers' for fishing across Victoria.

The river has a range of social values including fishing, boating, camping, swimming, picnicking and bushwalking. In an economic sense the rivers are valued for the supply of water for agriculture, drinking water, tourism and fishing.

Significant landscapes

Warrnambool contains areas of great natural beauty. The community values the city's landscape character. The ocean, the Merri River, the Hopkins River, inland hilltops and ridgelines, and surrounding rural areas can be viewed from numerous public areas and private dwellings throughout the city. Respect for the sharing of views, rather than necessarily the retention of all existing views, is an important issue.

Clause 21.03-2 seeks to ensure development sustains and enhances water quality and sediment loads. With regards to development of ridgelines, Clause 21.03-2 encourages the protection of prominent ridgelines.

Clause 21.06 'Built Environment and Heritage' identifies the appearance of the city's residential areas is important in maintaining a strong level of civic pride.

Clause 21.06-1 'Urban Environment' notes the siting and design of buildings can have a critical impact on visual appearance of urban streetscapes. Clause 21.06-1 outlines a number of Design and Development Overlays have been

implemented to recognise areas which require sensitive design solutions (including the Coastal Hopkins Growth Area).

Clause 21.07 'Housing' seeks to ensure new housing is located within close proximity to existing and/or planned transport corridors, activity centre and open space. A diverse range of housing should also be provided to meet community needs.

Clause 21.11-5 'Coastal Hopkins Growth Area' contains policy which guides decision making in the growth area. The vision for the Coastal Hopkins Growth Area is:

"To provide a superior quality residential environment that responds to local characteristics and context, and provides a range of living opportunities through a diversity of residential densities situated in a variety of landscape settings."

Strategies relevant to this proposal include:

General

- Ensure that any proposed use or development within the Coastal Hopkins Growth Area is generally in accordance with the Coastal / Hopkins River Structure Plan, including the Coastal Hopkins Growth Area Framework.

Land use and activities

- Encourage environmental housing with a substantial river edge reserve, north of Hopkins Point Road.

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Urban design and built form

- Encourage development that is clustered together to provide for large areas of public open space, environmental linkages and the protection of ridgelines and significant flora and fauna species.
- Encourage development densities that are not necessarily uniform but respond to a visual and environmental assessment of potential development impacts.
- Require building and site design to blend into the landscape.
- Require subdivision with buffer areas that afford effective protection of the environmental values of the coast and river environments from impacts of stormwater, etc.
- Protect the ridgeline, coastal and river systems when considering any form of development.
- Require the identification of building envelopes which respond to the visual and environmental context of the site and allow for the reasonable sharing of views to the ocean and/or the Hopkins River from adjoining properties.

Access and circulation

- Provide a network of pedestrian and bicycle paths that provide links between neighbourhoods, open space areas and to Hopkins Point Road.
- Road reserves are to contain significant space to provide a vegetated backdrop/foreground to future buildings and structures.
- Provide a movement network that responds to the topography of the site, and provides a permeable layout for convenient vehicle and pedestrian access.
- Ensure access points from Hopkins Point Road are carefully chosen to optimise sight distance in both directions.

Open space and landscape

- Preserve the environmental values and landscape qualities of the area.
- Require the preparation of detailed siting and design schemes such as stormwater reuse, drought resistant gardens, habitat creation and enhancement which includes revegetation and solar orientation of buildings that demonstrate net environmental benefits resulting from any form of proposed residential development.
- Retain the coastal dune and river frontages through designation as public reserves for biodiversity purposes.
- Designate the ridgeline north of Hopkins Point Road as public open space with appropriate setbacks to housing to maintain a vegetated ridgeline.
- Protect the ridgeline, coastal and river systems when considering any form of development.
- Actively encourage revegetation so as to reduce the visual impact of the height and built form of buildings and structures.

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3.3 Zones

General Residential Zone

The development plan area is zoned for residential purposes (General Residential Zone) which seeks to:

- To implement the Municipal Planning Strategy and the Planning Policy Framework.
- To encourage development that respects the neighbourhood character of the area.
- To encourage a diversity of housing types and housing growth particularly in locations offering good access to services and transport.
- To allow educational, recreational, religious, community and a limited range of other non-residential uses to serve local community needs in appropriate locations.

Use

Under the General Residential Zone, a planning permit is not required to use land for a 'Dwelling'. Uses such as 'Convenience shop', 'Food and drink premises', 'Leisure and recreation', 'Market', and 'Take away food premises', are Section 2, permit required uses. As set out in this development plan, these uses are encouraged to locate within the 'village centre'.

Subdivision

A planning permit is required to subdivide land. An application to subdivide land must meet the requirements of Clause 56 of the Warrnambool Planning Scheme.

Construction and extension of one dwelling of a lot

A permit is required to construct a dwelling on a lot less than 300 square metres in area. A permit is required to construct or extend a front fence if the fence exceeds the maximum height specified in Clause 54.06-2 of the Warrnambool Planning Scheme.

Construction and extension of two or more dwellings on a lot

A permit is required to:

- Construct a dwelling if there is at least one dwelling existing on the lot.
- Construct two or more dwellings on a lot.
- Extend a dwelling if there are two or more dwellings on the lot.
- Construct or extend a dwelling if it is on common property.
- Construct or extend a residential building.

Buildings and works associated with a section 2 use

A permit is required to construct a building or construct or carry out works for a Section 2 use.

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Public Park and Recreation Zone

The Public Park and Recreation Zone applies to the Hopkins River frontage. The Public Park and Recreation Zone seeks to:

- To implement the Municipal Planning Strategy and the Planning Policy Framework.
- To recognise areas for public recreation and open space.
- To protect and conserve areas of significance where appropriate.
- To provide for commercial uses where appropriate.

As set out in this development plan, land along the Hopkins River will be vested with Council for drainage and open space purposes. Land within the Hopkins River corridor should be rezoned Public Park and Recreation Zone once vested with Council.

Road Zone, Category 2

Hopkins Point Road is included within the Road Zone, Category 2. The purpose of the Road Zone is to *identify significant existing roads*.

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3.4 Overlays

Environmental Significance Overlay

Land with frontage to Hopkins River is included within the Environmental Significance Overlay with its purpose being:

- To implement the Municipal Planning Strategy and the Planning Policy Framework.
- To identify areas where the development of land may be affected by environmental constraints.
- To ensure that development is compatible with identified environmental values.

This development plan responds to the statement of environmental significance and environmental objectives contained within Schedule 2 to the Environmental Significance Overlay.

Design and Development Overlay

The development plan area is included within Schedule 17 to the Design and Development Overlay (DDO17 - Coastal/Hopkins River Environment Growth Area).

Of relevance to this development plan, DDO17 contains the following design objectives:

Vegetation

- To re-establish the vegetated setting for the site using indigenous coastal species.

Landscape Setting

- To ensure landscaping is designed and sited to provide views to the ocean.
- To re-establish vegetation dominated views of the area.

Views

- To provide for the reasonable sharing of views to the ocean, coastal dunes, river and the surrounding landscape.
- To minimise the visual impact of development from key public viewing locations.

Siting

- To ensure buildings and structures are sited so as to minimise visual intrusion on views to the ocean.
- To ensure buildings and structures are sited to avoid steep slopes.
- To integrate buildings and works into the river corridor and rural dune landscapes.
- To minimise the visual impact of development along the ridgeline.
- To ensure buildings and structures are strategically sited so as to maximise the degree to which the development is absorbed in the landscape.

Height and building form

- To ensure development is designed to allow views across and between buildings to the ocean.
- To ensure buildings and structures sit within the coastal dune landscape and vegetation setting.

Site coverage

- To develop a spacious building setting with space for planting.
- To minimise the dominance of car parking structures and outbuildings.

Environmentally Sustainable Design

- To encourage good environmentally sustainable practices including maximising solar access and rainwater harvesting.
- To incorporate the use of Water Sensitive Urban Design principles.

Materials and design detail

- To ensure buildings demonstrate a high standard of contemporary design and complement the coastal setting.

A planning permit is required to construct a building or carry out works, including the following:

- Construction of a front fence which is not post and wire construction.
- Where land adjoins a public reserve (such as the river reserve, open space, pedestrian links), a permit is required to construct a boundary fence which adjoins the reserve and is not of post and wire construction.
- Where land adjoins land not covered by this overlay (i.e. land to the west of the development plan area), a permit is required to construct a boundary fence which is not post and wire construction.
- Construction of a tennis court or swimming pool.

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Development Plan Overlay

The development plan area is included within Schedule 13 to the Development Plan Overlay.

The purpose of this overlay is to ensure subdivision and development within the growth area is generally in accordance with the layout, design and density objectives of Coastal / Hopkins River Structure Plan.

A development plan must address the following elements:

- Subdivision and building envelopes
- Movement network
- Open Space and landscape
- Service provision.

The objectives and requirements contained in Schedule 13 to the Development Plan Overlay are set out in following section.

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4. Schedule 13 to the Development Plan Overlay

The Coastal / Hopkins River Structure Plan provides the strategic basis for the design and development provisions of Schedule 13 to the Development Plan Overlay.

The Coastal / Hopkins River Structure Plan contains to the following vision: (emphasis added)

"The Coastal / Hopkins River Environment provides a **superior quality residential environment** that responds to local characteristics and context. The area functions as a neighbourhood of Warrnambool and provides a range of living opportunities through a **diversity of residential densities** situated in a variety of landscape settings.

A **strong sense of community** is created by a **network of walking and bicycle paths** that provide links between the open spaces and residential areas. **Convenient access to shops and services** is available to many, with a concentration of housing opportunities located close to Hopkins Point Road, and more spacious living opportunities located in environmentally and visually sensitive areas.

The **residential area sits in harmony with the existing landscape** with roads and buildings located to minimise alterations to the natural topography. Looking back from the surrounding areas, the Coastal / Hopkins River Environment appears as **heavily vegetated** with dwellings just visible between trees and open spaces, and a low-lying **vegetated ridge forming the skyline**.

Coastal, estuary and river views are available for everyone to enjoy with the **public open spaces located at the primary vantage points** in the area. The careful siting and design of buildings ensures that **views are available between buildings, within streets, and from residential allotments**.

There is a focus on protecting and enhancing the local environment throughout the area. **Hopkins River and its environs is revegetated and incorporated into public open space**, as is Logans Beach, the coastal fringe and the ridge area. Improvements to water quality are achieved through appropriate stormwater management filtration systems in streets and open spaces, and through best practice environmentally sustainable development within private allotments."

4.1 Objectives of the Development Plan

Schedule 13 to the Development Plan Overlay seeks to ensure development plans address the following objectives:

Overall objectives

To implement the vision of the Coastal Hopkins River Environment Structure Plan to create a quality residential environment that responds to local characteristics and context, and provides a range of living opportunities through a diversity of residential densities situated in a variety of landscape settings, with specific reference to the following plan elements:

- Subdivision and building envelopes.
- Movement network.
- Open space and landscape.
- Service provision and drainage.

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Subdivision and building envelope objectives

To provide a neighbourhood that:

- Provides diversity in lot sizes and housing styles, with smaller lots in key, high amenity locations and larger lots in appropriate locations such as on steep slopes.
- Ensures future buildings and structures can be sited to incorporate space for the planting of substantial vegetation and avoids steep slopes.
- Has wide nature strips to allow planting that dominate the roadside setting.
- Incorporates the use of Water Sensitive Urban Design principles.

Design considerations:

- The development plans provides a diversity of lot sizes with smaller lots located on areas of less slope and near higher amenity locations (such as open space and the 'village centre').
- Lower density housing has been located in areas of steep slope.
- Lot sizes allow space for substantial vegetation.
- Roads incorporate wide nature strips to allow substantial vegetation.

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Movement network objectives

To provide a movement network, including a connector road, local street and pedestrian/cycle path network that:

- Responds to the topography.
- Establishes a permeable street network which allows for safe and convenient pedestrian, bicycle and vehicle movement.
- Enhances pedestrian and bicycle links between public open spaces within and beyond the study area and between existing and future residential development in the Coastal / Hopkins River Environment area.
- Ensures that development is designed to be integrated with existing public transport systems.
- Manages the impacts of residential development on the existing road network.

Design considerations:

- The development plan provides a permeable network of roads, and walking and bicycle paths which respond to topography and enable people to access open space and public transport.
- Roads, walking and cycling paths have been limited in areas of steep slope.
- Road, pedestrian and bicycle connections have been provided to future neighbourhoods, east of the development plan area.
- Access arrangements to Hopkins Point Road have been designed to ensure there is no impact on road safety.

Open space and landscape objectives

To provide an interlinked open space network that:

- Provides accessible and high amenity public open spaces incorporating existing landscape and environmental features and takes advantage of key views.
- Provides the opportunity for views of the surrounding landscape from private allotments, and public domain.
- Minimises the visual intrusion of residential development when viewed from key locations in the surrounding areas.
- Provides pedestrian links in open space to adjoining areas.

Design considerations:

- The development plan provides a network of open space which builds upon the existing network of open spaces within the surrounding area.
- Open spaces will be quality spaces which incorporate existing landscape and environmental features (such as the river corridor and ridgeline).
- Lots have been oriented to capture key views. Open spaces have been located to take advantage of primary vantage points.
- Lot sizes allow space for substantial vegetation.
- Roads incorporate wide nature strips to allow substantial vegetation.

Service provision and drainage objectives

To provide physical services and infrastructure that:

- Meets the needs of the future community and the development.
- Provides for the efficient, staged delivery of services and infrastructure to ensure all lots are provided with adequate services.
- Incorporates a Water Sensitive Urban Design approach to stormwater management to protect water quality.

Design considerations:

- The development plan provides efficient delivery of services and infrastructure which improves environmental performance of proposed subdivisions.

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4.2 Requirements for the development plan

The development plan must include a detailed site analysis and design response which addresses the requirements set out in Schedule 13 to the Development Plan Overlay.

The **site analysis** must include the following:

- An **environmental assessment** of the flora, fauna and habitat significance of the land which includes recommended actions for management, revegetation and restoration of any identified conservation and vegetation protection areas where relevant. The assessment must also make recommendations with regard to management of noxious weeds as identified by the *Catchment and Land Protection Act*.
- An archaeological survey and heritage assessment which includes recommendations for the protection, restoration and interpretation of significant sites, and where appropriate, design measures to sensitively integrate sites. The assessment must also identify areas where a **Cultural Heritage Management Plan** is required by the *Aboriginal Heritage Act*.
- A **landscape assessment** that defines any important landscape views or vistas and any landscape features.
- An **environmental audit** identifying any environmental hazards or contamination on the land and proposed treatments, if any; or a qualified statement indicating the absence of such hazards or contamination.
- A consolidated **site analysis** plan in digital and hard copy format that depicts all relevant site analysis information.

- A **geotechnical assessment** (Functional design) which incorporates at least the following information:
 - Location of proposed lots,
 - Proposed services, such as sewer, stormwater, water, gas, electricity, telephone and other utility services,
 - Contour lines to AHD (500mm vertical intervals),
 - Earthwork details, and
 - Identification of trees to be removed or retained.
- A **geotechnical site analysis** that includes a:
 - Stability Assessment Report; and
 - Landslide Hazard Rating Analysis.

The site analysis is set out in Section 5 of this report. In addition to the site analysis requirements, the site analysis sets out property ownership, surrounding land uses, and existing access and linkages.

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The development plan must also include a **design response** based on the site analysis. The report must include:

- A street layout plan, including typical cross sections and details of public transport and pedestrian / bicycle paths.
- Indicative lot layout plan, including typical building envelopes.
- An open space plan, including a biodiversity corridor along the Hopkins River edge, and open space corridor along the ridgeline, landscape masterplans for each open space area, and a street tree theme.
- An assessment of how stormwater will be collected, treated and managed.

The design response is detailed in Section 6 and in the accompanying Stormwater Management Plan (TGM Group).

The following reports and assessment have informed the preparation of the development plan:

Environmental assessment

Flora and Fauna Assessment and Net Gain Analysis
Ecology Partners Pty Ltd, June 2011

Cultural Heritage Management Plan

Hopkins Point Road Warrnambool: Residential Subdivision, Cultural Heritage Management Plan, Number 11721
Urban Colours Art and Cultural Heritage Consultants, 2 September 2011
(approved 7 October 2011)

Functional design

Functional design, Drawing Number 18311-03
TGM Group Pty Ltd, March 2019

Geotechnical site analysis

Geotechnical Investigation
A.S. James Pty Ltd, 30 May 2012

Services and drainage

Site Stormwater Management Plan
TGM Group Pty Ltd, August 2018

Traffic

Traffic Engineering Assessment, Proposed Residential Subdivision as 'Madden Lane' Hopkins Point Road, Warrnambool
Traffix Group Pty Ltd, September 2018

Environmental Assessment

Preliminary Site Investigation at Hopkins River Residential Growth Area, Warrnambool
Environmental Earth Sciences, November 2018

The Functional Design (Drawing Number 18311-03) forms part of this approved development plan.

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5. Site analysis, issues and opportunities

5.1 Property ownership

The development plan area comprises two landholdings ranging from 3 hectares to 75 hectares.

The largest landholding (Landholding 1) includes three titles across 75 hectares of land. A planning permit (Planning Permit No. PP2018-0128) was recently granted enabling the land to be subdivided to create three 'mega-lots' capable of further subdivision.

The landholding has approximately 930 metres frontage to Hopkins Point Road and approximately 870 metres frontage to the Hopkins River. Approximately 4.4 hectares of the landholding is located within the Hopkins River. The landholding contains a former dairy, internal fencing and planted shelter belts

The remaining landholding (Landholding 2) is located in the south-eastern corner of the development plan area. The landholding has approximately 215 metres frontage to Hopkins Point Road with an overall area of 3 hectares. The land is developed with a modern double storey dwelling and associated outbuildings.

Design considerations:

- The development plans allows individual landowners opportunity to develop independently of each other without compromising the development plan.
- The development plan provides opportunities for landowners to retain existing dwellings within the development of their land.

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LEGEND

- Development Plan Area
- Property Boundary
- Existing Buildings
- Hopkins River



Figure 2: Property ownership

5.2 Surrounding land uses

The development plan area is bounded by the Hopkins River to the north and low density residential development to the south and west.

Land to the east is included within the General Residential Zone and is progressively being developed for residential purposes in accordance with the approved development plan for that part of the growth area (North Side of Hopkins Point Road, Warrnambool Development Plan, TGM Group, 2014).

The *North Side of Hopkins Point Road Development Plan* facilitates the development of 78 allotments in three stages (Hopkins Ridge Estate). To-date, 54 lots have been developed with several dwellings constructed and/or currently under construction. The development plan includes a 4,000 square metre central park and a diverse range of residential lots sizes (600 square metres to 3,000+ square metres). Two road connections are planned to connect the Hopkins Ridge Estate with the Hopkins Heights Development Plan area (one road connection has been constructed). Road, pedestrian and bicycle connections to this area have been considered through the preparation of the development plan.

Warrnambool City Council recently adopted a framework plan for the Logans Beach area (*Logans Beach Strategic Framework Plan, 2017*). Land to the south and west of the development plan area is included within the framework plan.

The framework plan provides guidance on planning controls to facilitate further residential development within the Logans Beach area. Of relevance to this development plan, the framework plan seeks to facilitate an additional 200 -250 within the Logans Beach area (excluding the Motang Estate).

The Coastal / Hopkins River Structure Plan identifies an opportunity to provide small-scale retail facilities to service future population growth within the area. The provision of a range of limited non-residential uses (such as cafes, restaurant, milk bar, medical centre, etc.) should be considered in the preparation of the development plan.

Design considerations:

- The development plans provides road, pedestrian and bicycle connections to future neighbourhoods, east of the development plan area.
- The development plan provides for a range of limited non-residential uses to service future needs of the growing community (Hopkins Heights 'village centre').

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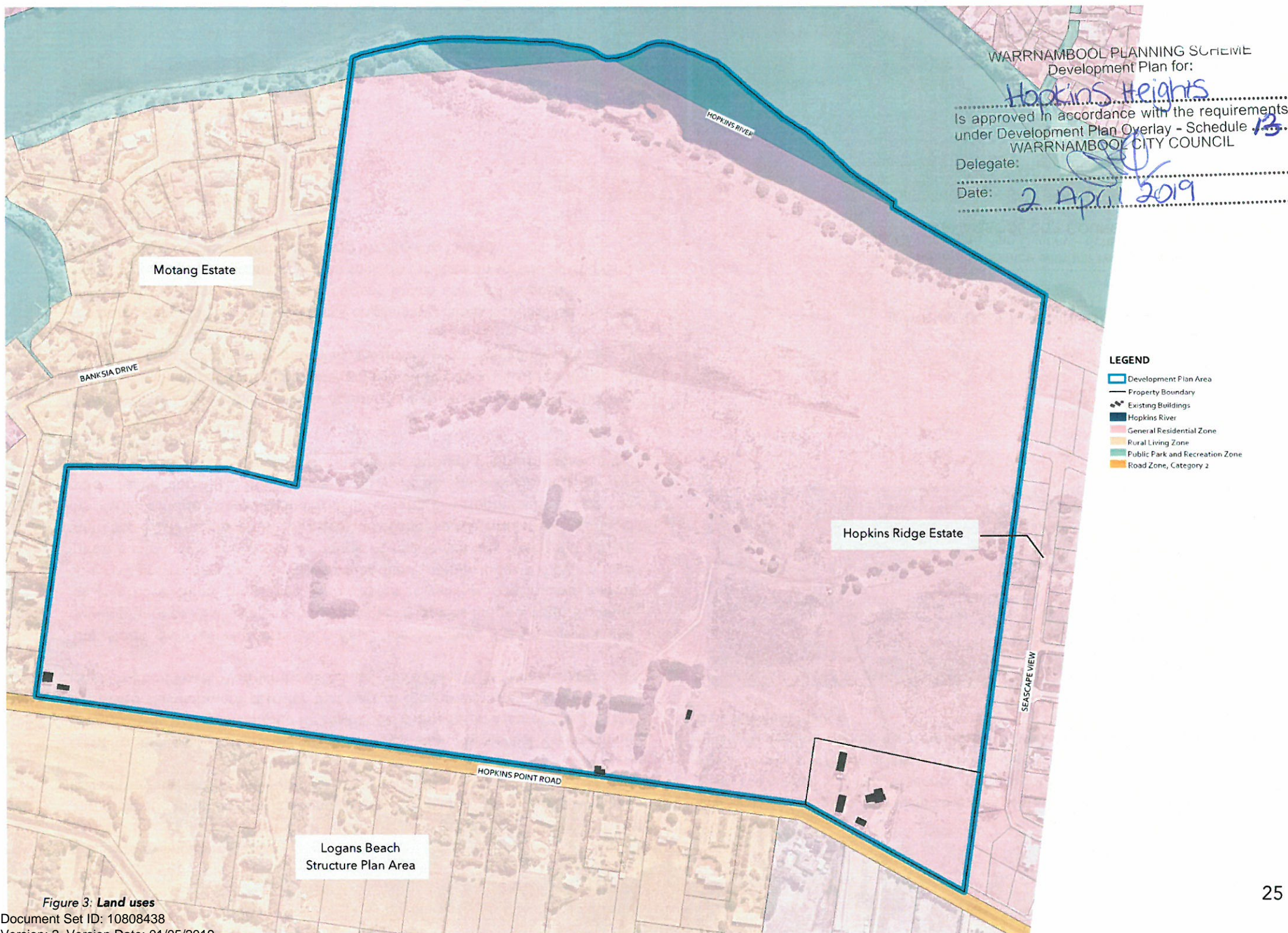


Figure 3: Land uses

Design considerations:

- The development plan provides a permeable network of roads, and walking and bicycle paths which respond to topography.
- Roads, walking and cycling paths have been limited in areas of steep slope.
- Roads incorporate wide nature strips to allow substantial vegetation.
- The development plan provides a diversity of lot sizes with smaller lots located on areas of less slope and near higher amenity locations (such as open space). Lower density housing has been located in areas of steep slope.
- Lots have been oriented to capture key views. Open spaces have been located to take advantage of primary vantage points.
- Extensive planting will be provided along the ridgeline.
- Building height have been limited near the ridgeline to maintain a vegetated ridgeline to provide a 'green ridge' to the estate.
- With regards to sharing of views, the development plan provides that dwellings are to be staggered to ensure coastal / river views from dwellings in Banksia Drive and Acacia Court are maintained.

Guidelines for planning permit applications:

- A Slope Management Plan should be submitted with a planning permit application for subdivision and include:
 - Site description and design response.
 - Statement describing how any land with pre-development slopes over 20% will be subdivided.
 - A statement and/or diagrams detailing the proposed landscape and urban design outcomes that will be achieved to complement the slope.
 - A geotechnical report completed by a suitably qualified engineer to confirm the stability of the slope and assessment of risks posed by the site conditions and proposed earthworks and drainage.
 - Proposed road cross sections and long sections to demonstrate how slopes over 20% are being responded to through design.
 - Details of all proposed batters, cut and fill earthworks, retaining walls, driveway crossover locations and drainage solutions required for the subdivision of land that includes an existing pre-development slope greater than 20%.
 - Building envelopes (or an alternate design response) to respond to the slope management methods utilised.

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5.3 Topography and views

The development plan area includes a prominent hill which falls towards Hopkins Point Road to the south and the Hopkins River to the north. The highest point within the development plan area is approximately 65 metres above sea-level, one of the highest points in Warrnambool.

The development plan area includes a ridgeline which runs in an east-west direction. Where appropriate, existing vegetation on the ridgeline is to be retained and included within the 'linear park'. This vegetation, as well as a tree plantings contained within a generous 22 metre road reserve, will screen housing from significant viewing locations outside the estate. All buildings on the site will be sited below the height of vegetation on the ridgeline.

From this point, the topography falls to the north, south and west with a slope between 5% and 10%. There is a dramatic increase in slope (between 20% and 35%) down to the edge of the Hopkins River with some extreme areas of slope close to the bank of the river. There is also a steep sloping area in the south-west corner of the development plan area. Some parts of this area have slope greater than 20%.

These slopes require special consideration and a tailored approach to address matters concerning street alignment, bicycle and pedestrian accessibility, lot layout and dwelling design. These matters will be addressed thoroughly at the planning permit stage and through detailed design.

Because of the high elevation, the development plan area has a variety of long-distance panoramic views. Areas north of the ridgeline have views of the Hopkins River and long-distance rural views, including long-distance views to Mount Warrnambool to the north-east. South of the ridgeline, panoramic views are available of urban Warrnambool, the Hopkins River mouth and the ocean.

Long-distance views are possible to Port Fairy, across the Warrnambool Breakwater / harbour precinct.

Parts of the development plan area are highly visible from nearby viewing locations such as Moyjil / Point Ritchie, the whale watching platform car park, and the Deakin University boat ramp.

A number of dwellings in the Motang Estate (Banksia Drive) have been constructed to take advantage of views of the coast and the Hopkins River Mouth. Development adjacent to Banksia Drive will need to consider existing views lines.

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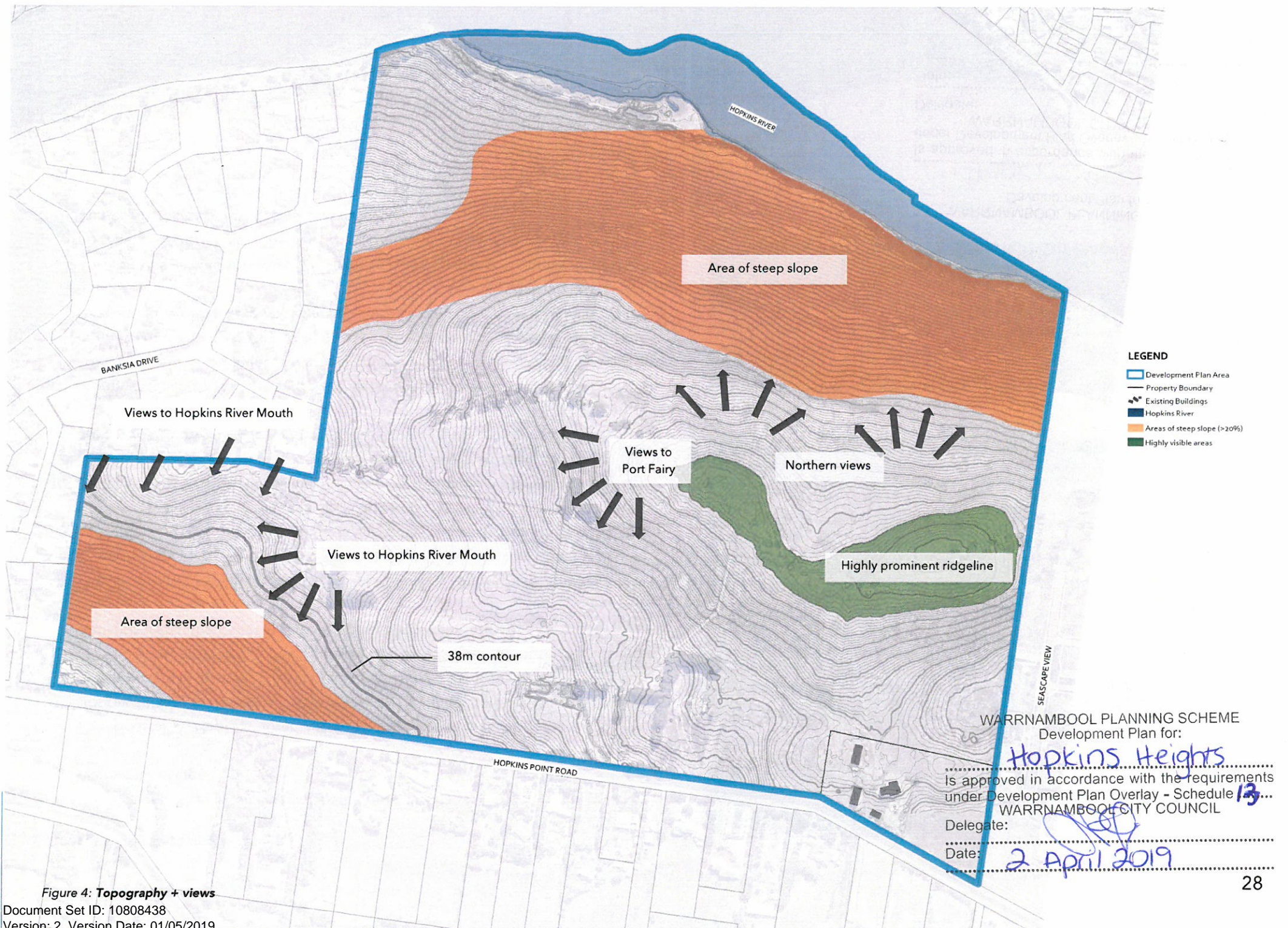
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5.4 Waterways and drainage

The Hopkins River is an integral part of the drainage corridor in the greater Warrnambool area.

The Coastal / Hopkins River Structure Plan identifies discharge of urban stormwater into the Hopkins River will need to be treated through Water Sensitive Urban Design measures which protect the quality of the river. The structure plan recommends stormwater management plans consider the use of existing wetlands within the Water Sensitive Urban Design measures.

The Stormwater Management Plan (TGM, August 2018) which accompanies this development plan, identifies the development plan area has three drainage catchments. The northern catchment currently discharges north toward the Hopkins River via a combination of concentrated and sheet flow from the central ridgeline. This 'Plan' identifies that the estate is able to be suitably drained with a number of scenarios/options which will be fully explored and resolved in the approved Stormwater Management Plan.

Stormwater runoff from the southern catchment discharges via a combination of concentrated and sheet flow towards Hopkins Point Road. Drainage infrastructure within Hopkins Point Road consists of an underground network and overland flow within kerb and channel which convey flows to the Hopkins River.

The Hopkins River is also an area of high amenity and presents significant passive recreation opportunities. The structure plan recommends that the river corridor be designated as open space and revegetated.

Design considerations:

- Roads have been aligned to allow for overland flows along natural drainage lines.
- Water sensitive urban design infrastructure and techniques will be utilised in road reserves, public reserves and elsewhere to appropriately treat stormwater before discharge.
- The final stormwater drainage discharge option/s will be determined in the approved Stormwater Management Plan.
- Rooftop stormwater discharge will be minimised through a requirement for each house to retain stormwater as drinking water in fresh water tanks.
- A biodiversity corridor has been provided along the entire length of the Hopkins River.
- Access has been provided to the Hopkins River corridor in the form of a shared path and bicycle path.

Guidelines for planning permit applications:

- Applications for subdivision must be accompanied with a Stormwater Management Plan which identifies drainage requirements (including interim measures) for each stage of subdivision.

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5.5 Flora and fauna

Land within the development plan area has been subject to modification through agricultural use and is dominated by exotic vegetation (open pasture).

Vegetation within the Hopkins River corridor comprises native (non- indigenous) and exotic trees and shrubs, with a dense ground cover of exotic weed species.

Paddocks within the development area are dominated by exotic vegetation and contain patches of Striped Wallaby-grass *Austrodanthonia racemose* var. *racemose* which have colonised (i.e. not remnant vegetation) as the paddocks have historically been cultivated, fertilised and grazed. The Flora and Fauna Assessment concluded areas supporting colonised Striped Wallaby-grass were not considered representative of the original vegetation type (i.e. Damp Sands Herb-rich Woodland) and do not support habitat for rare or threatened flora or fauna species. While a permit is required to remove or disturb native grasses, the assessment concluded a Net Gain analysis was not required as no remnant vegetation was recorded within the development plan area.

No *Environment Protection and Biodiversity Conservation Act 1999* listed flora or fauna species were recorded during the field assessment. The assessment concluded that none are considered likely to occur within the development plan area as no suitable habitat is present.

A number of weed species listed as noxious under the *Catchment and Land Protection Act 1994* were recorded during the site assessment (such as Variegated Thistle, Soursob, and African Box Thorn).

To mitigate / ameliorate potential impacts on ecological values, the assessment recommended the following measures:

- Where development is proposed adjacent to areas of ecological value (Hopkins River), these sites should be fenced and identified as 'no go' areas to avoid disturbance during construction.
- A Weed Management Plan should be developed to control weeds during construction.
- Soil disturbance and sedimentation near rivers or drainage lines should be kept to a minimum to avoid / minimise impacts on fauna habitats.
- Ensure best practice sediment and pollution control measures are undertaken to minimise disturbance to rivers and drainage lines.
- Use indigenous plants associated within the relevant EVC (Damp Sands Herb-rich Woodland) as part of any landscaping works to increase habitat for flora and fauna.

Design considerations:

- A biodiversity corridor has been provided along the entire length of the Hopkins River.

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Figure 5: Waterways + drainage

Guidelines for permit applications:

- Applications for subdivision within areas where Striped Wallaby-grass has been identified, must be accompanied with a flora and fauna assessment in accordance with *Guidelines for the removal, destruction or lopping of native vegetation 2017*.
- Where development is proposed adjacent to areas of ecological value (Hopkins River), these sites should be fenced and identified as 'no go' areas to avoid disturbance during construction.
- Soil disturbance and sedimentation near rivers or drainage liens should be kept to a minimum to avoid / minimise impacts on fauna habitats.
- Applications should ensure best practice sediment and pollution control measures are undertaken to minimise disturbance to rivers and drainage lines.
- Applications for subdivision must be accompanied with a Weed Management Plan to controls weeds during construction.

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Area containing
Striped Wallaby-grass

Cypress windrow
recently removed

LEGEND

- Development Plan Area
- Property Boundary
- Existing Buildings
- Hopkins River
- Existing Vegetation



Figure 6: Flora + fauna

5.6 Geology and soils

The Geological Survey of Victoria, 1:250,000, Colac Sheet identifies the development plan area to be underlain by Tertiary Limestone, locally known as Port Campbell Limestone. Weathering of the limestone typically results in shallow surface residual silts underlain by firm to very stiff residual clays, which grade to variably weathered limestone. The residual clays are generally highly reactive, and the depth and lateral extent of rock is often highly variable.

The Geotechnical Investigation identified the development plan areas is located on a limestone ridge which rises approximately 40 metres above the level of the river. Land to the north and south-west contains steep slopes (approximately 15-23 degrees) with less slope (less than 10 degrees) generally across the rest of the development plan area.

The development plan area has a number of areas of shallow instability. The Geotechnical Investigation confirms deeper subsurface limestone is not subjected to slope failures. Accordingly, the investigation concluded deeper foundation arrangements, together with good hillside practice management, would negate any risk of shallow failures at the site.

The Geotechnical Investigation includes recommendations relating to:

- Cut and fill earthworks;
- Short-term excavations;
- Removal of vegetation and revegetation;
- Site drainage;
- Design and construction requirements for roads; and
- Further detailed geotechnical investigations, including risk assessments and slope stability model for lots located in the northern parts of the development plan area.

Subdivision and development must be undertaken in accordance with the requirements set out in the Geotechnical Investigation.

Guidelines for permit applications:

- Applications for subdivision and development (dwellings) in areas of steep slopes must be in accordance with the requirements set out in the Geotechnical Investigation.

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5.7 Cultural Heritage

The development plan is within an area of cultural heritage sensitivity as described in the *Aboriginal Heritage Regulations 2007*. A Cultural Heritage Management is required to be prepared and approved by the relevant Registered Aboriginal Party (or the Office of Aboriginal Victoria) prior to land being subdivided or developed.

A Cultural Heritage Management Plan has been prepared for the development plan area. No Aboriginal archaeological sites were identified within the development plan area. The Cultural Heritage Management Plan was approved by the Office of Aboriginal Victoria on 7 October 2011.

The management plan sets out actions in the event any Aboriginal cultural heritage sites are located during construction which will be followed during construction of the development.

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5.8 Access and linkages

Hopkins Point Road is a local connector road which runs in an east-west direction between Tooram Road to the east and the Hopkins River Bridge to the west and provides a connection to central Warrnambool. In the vicinity of the development plan area, Hopkins Point Road contains a 9.5 metre wide pavement with a single marked traffic lane in each direction. The pedestrian / bicycle network is currently limited to the existing road network. A key vision of the Coastal / Hopkins River Structure Plan is to create a strong sense of community through a network of walking and bicycle paths which link open spaces and residential area. A number of infrastructure upgrades (DCP projects) are outlined in a s173 Agreement registered on title comprising intersection works at Hopkins Point Road and Marfell Road intersections, Hopkins Road and Otway Road intersection and Flaxman street and Nicholson Street intersection.

Of relevance to this development plan, the structure plan sets out the following pedestrian / bicycle network:

- A shared pedestrian and bicycle trail in the centre of the growth area which commences at Hopkins Point Road and extends down to the riverside open space corridor.
- A shared trail along the riverside open space corridor.
- A shared trail that provides a scenic journey along the ridgeline and connecting the riverside open space and on-street pedestrian / bicycle network.
- An on-street pedestrian bicycle network connecting to the shared trails and open spaces to future residential areas.
- All roads will incorporate pedestrian footpaths.

With regards to roads, the structure plan seeks to provide a permeable road network which responds to topography. Internal roads are encouraged to follow natural contours to avoid excessively steep sections and reduce the visual impact of roads.

The structure plan encourages a centrally located avenue with access from Hopkins Point Road. The road is to incorporate substantial landscaping and a shared pedestrian / bicycle trail. A secondary access point is encouraged in the south-west corner of the development plan area which is intended to service properties in that immediate area. There are no existing bus routes which service the development plan area. The closest existing bus route runs directly past the western side of the Hopkins River Bridge. Discussions with Public Transport Victoria confirmed future subdivision within the development plan area should be designed in a way which accommodates a future bus route with the majority of allotments located within 400 metres of the route.

The Motang Estate, located to the west of the development plan area, comprises 32 rural residential lots with a private road (Banksia Drive). Banksia Drive directly adjoins the western boundary of the development plan area. The Panel which considered Amendment C76 identified:

"There is a considerable constraint to achieving a linkage to the west due to the privatised nature of roads in the Motang Estate and it is understood that submitters from the Motang Estate do not want linkages."

Accordingly, access to Banksia Drive should be restricted.

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Design considerations:

- The development plan provides a permeable network of roads, and walking and bicycle paths which respond to topography and enable people to access open space and public transport.
- Road, pedestrian and bicycle connections have been provided to future neighbourhoods, east of the development plan area.
- A bus route has been provided for within the Hopkins Heights development.
- Access arrangements to Hopkins Point Road have been designed to ensure there is no impact on road safety with basic right-turn treatment intersections to be constructed and a north side slip lane to the major entrance to the Hopkins Heights development.
- Vehicle access to Banksia Drive has been restricted.

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Hopkins Heights

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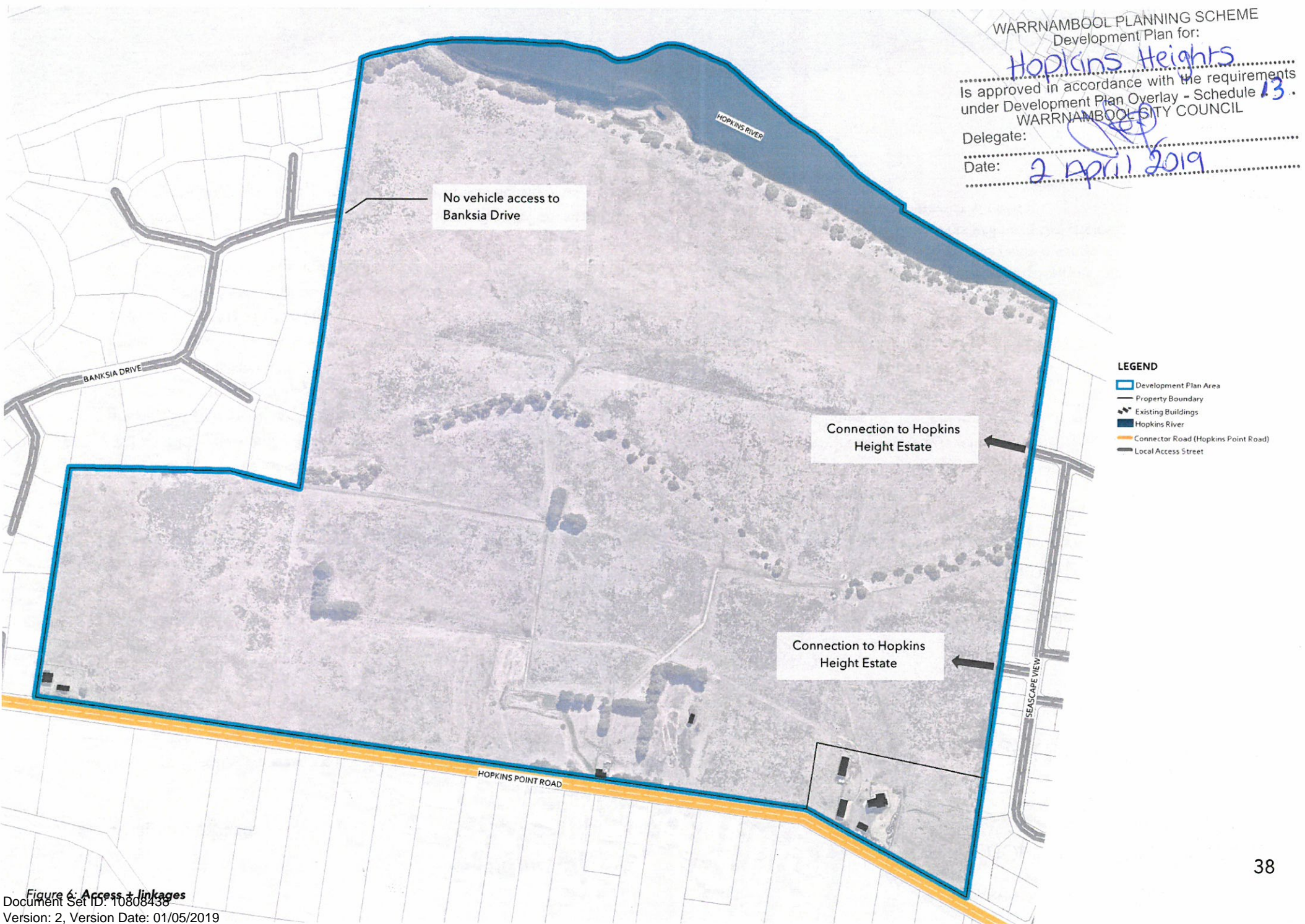
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LEGEND

- Development Plan Area
- Property Boundary
- Existing Buildings
- Hopkins River
- Connector Road (Hopkins Point Road)
- Local Access Street

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6. Design response

The Hopkins Heights Development Plan provides a cohesive design which responds to the Design Considerations contained in the site analysis section of this report.

The development plan seeks to:

- Provide a highly permeable internal road network which responds to topography and encourages walking and cycling throughout the development plan area.
- Provide road, pedestrian, bicycle connections to the Hopkins Ridge Estate (located to the east).
- Provide safe access and egress to Hopkins Point Road.
- Provide a diversity of lot sizes with smaller lots located on areas of less slope and near higher amenity locations (such as open space) and lower density housing located in areas of steep slope.
- Orientate lots to capture key views and locate open spaces to take advantage of primary vantage points.
- Provide sufficient space for significant tree planting within lots, road reserves and open spaces to form a landscaped backdrop to the neighbourhood.
- Provide a mixed-use neighbourhood centre to meet the day-to-day needs of the community with convenient access to residents ('Hopkins Heights village centre').
- Provide small green spaces within the local street network ('pause places') to foster community interaction and encourage physical activity.
- Design with topography to reinforce perception of topography, reduce need for earthworks and for drainage purposes.
- Protect and enhance ecological values of the Hopkins River corridor.
- Provide public access to the Hopkins River corridor through the provision of vehicle access to a car parking area in the north west corner of the site and a shared riverfront path.
- Enable individual land owners to develop independently of each other without compromising the development plan.
- Provide options for landowners to retain existing dwellings within the development of their land.
- Restrict access to Banksia Drive.
- Ensure key infrastructure (such as water supply and sewerage services) are provided in a timely and efficient manner.

As set out in the 'A day in the life at Hopkins Heights', the Hopkins Heights Estate will be is a great place to live with a range of housing choices. Everything is close by and residents will have the choice of walking, cycling or using public transport for their daily needs.

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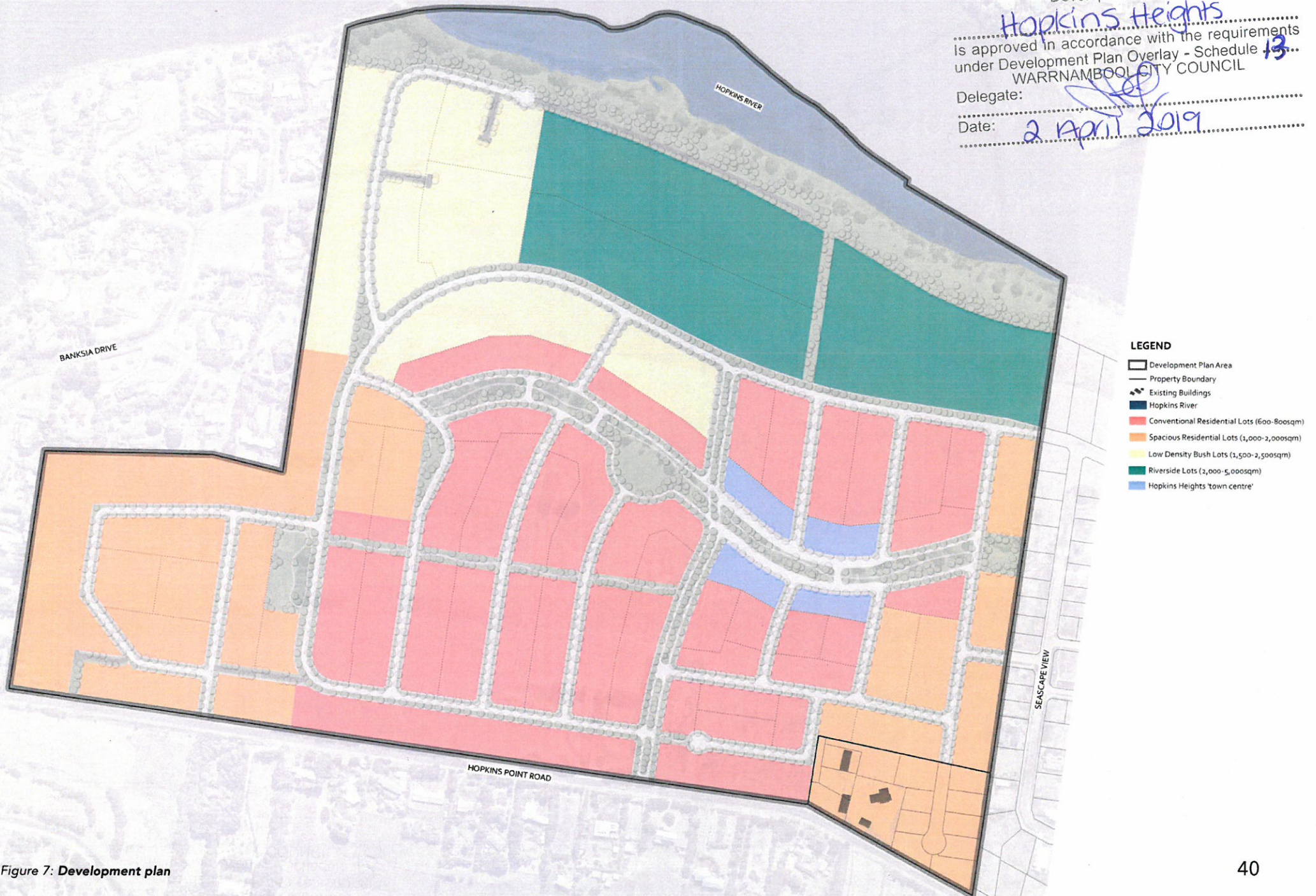


Figure 7: Development plan

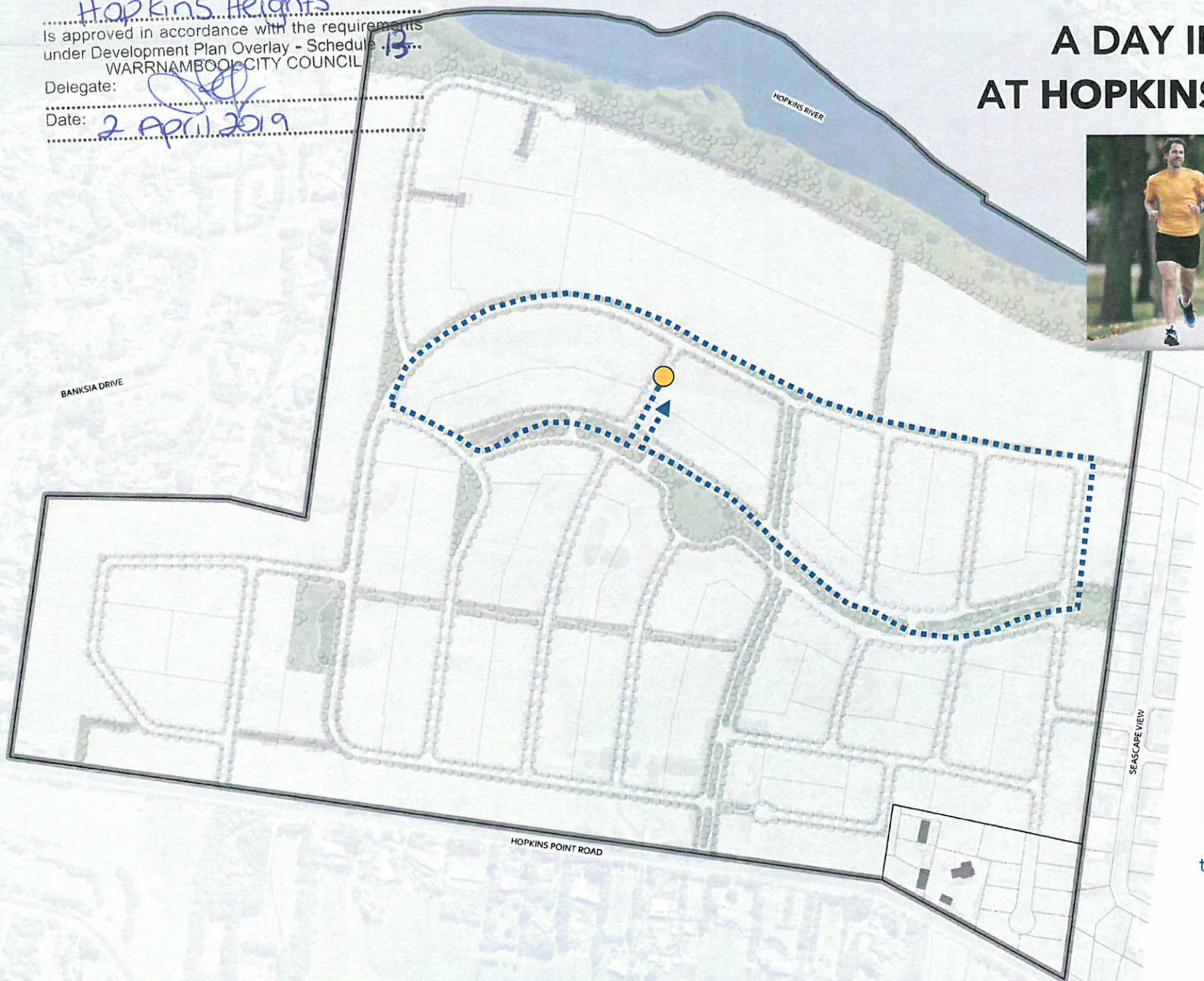
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A DAY IN THE LIFE AT HOPKINS HEIGHTS



7:30am

Morning jog or stroll with
the family dog through
the neighbourhood parks

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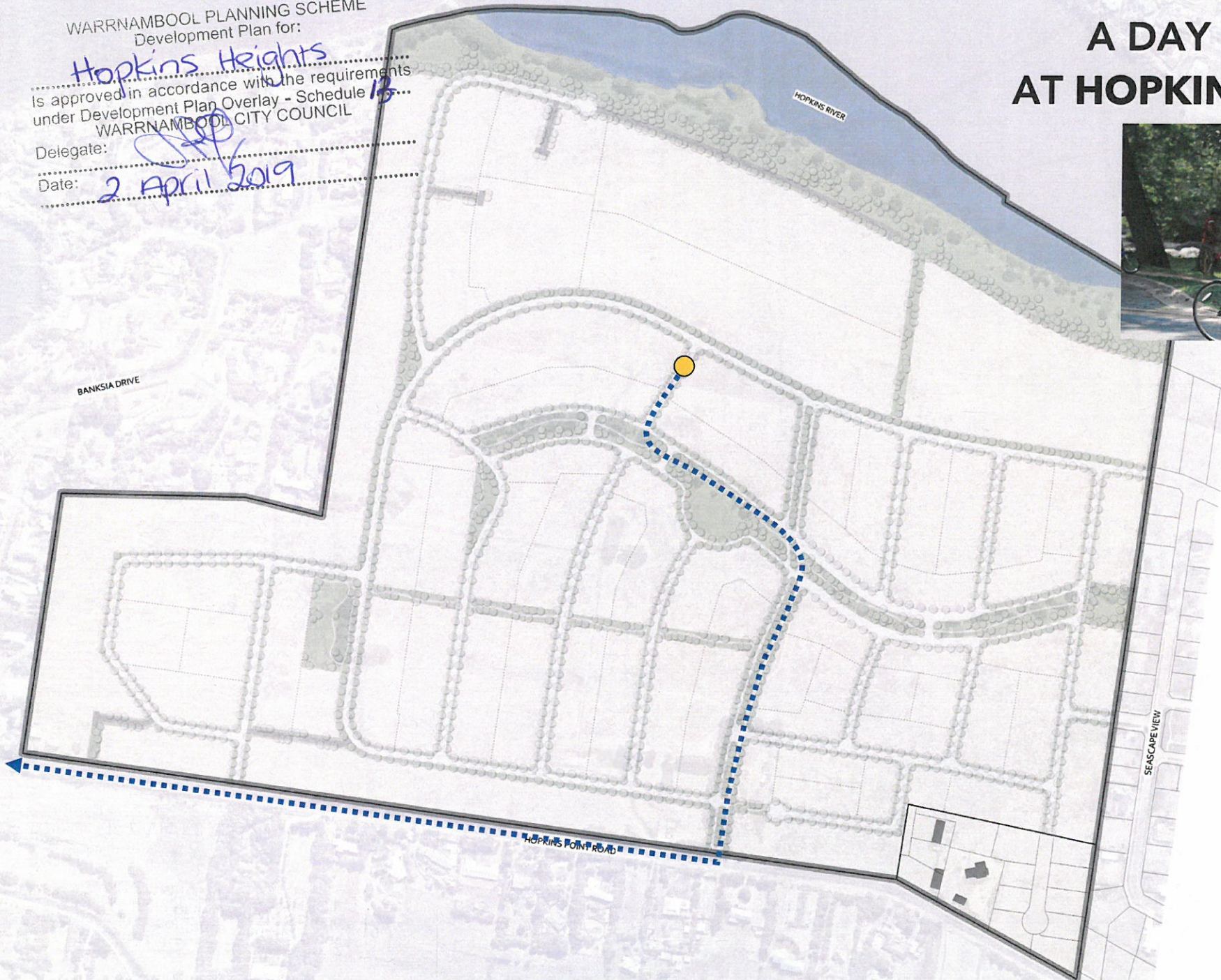
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A DAY IN THE LIFE AT HOPKINS HEIGHTS



7:30am
Morning jog or stroll with the
family dog through the
neighbourhood parks

8:30am
Walk to the bus with
kids and catch the bus
to school. Cycle
through the linear
park and into
Warrnambool CBD
for work

WARRNAMBOOL PLANNING SCHEME
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A DAY IN THE LIFE AT HOPKINS HEIGHTS



7:30am

Morning jog or stroll with the family dog through the neighbourhood parks

8:30am

Walk to the bus with kids and catch the bus to school. Cycle through the linear park and into Warrnambool CBD for work

11:30am

Walk to the 'village centre' shops for fresh produce, complete daily tasks and catch up with friends over coffee. Walk across the central plaza and enjoy brunch in the sun

Hopkins Heights

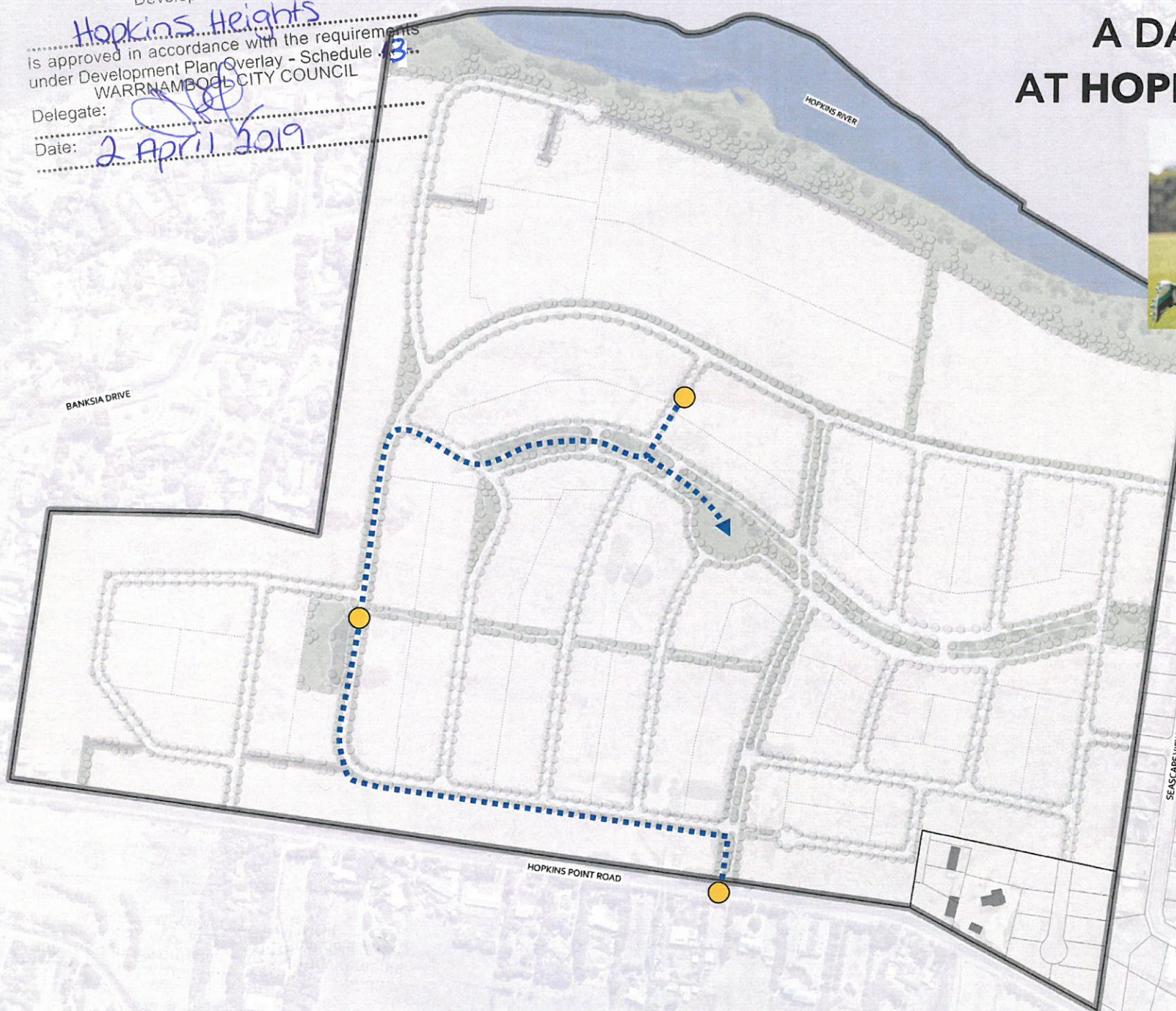
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A DAY IN THE LIFE AT HOPKINS HEIGHTS



7:30am
Morning jog or stroll with the family
dog through the neighbourhood
parks

8:30am
Walk to the bus with kids and catch
the bus to school. Cycle through the
linear park and into Warrnambool
CBD for work

11:30am
Walk to the 'village centre' shops for
fresh produce, complete daily tasks
and catch up with friends over
coffee. Walk across the central plaza
and enjoy brunch in the sun

3:45pm

Meet kids at the bus
stop after school and
return home via park.
Cycle to local park and
kick footy with the kids

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Development Plan for:

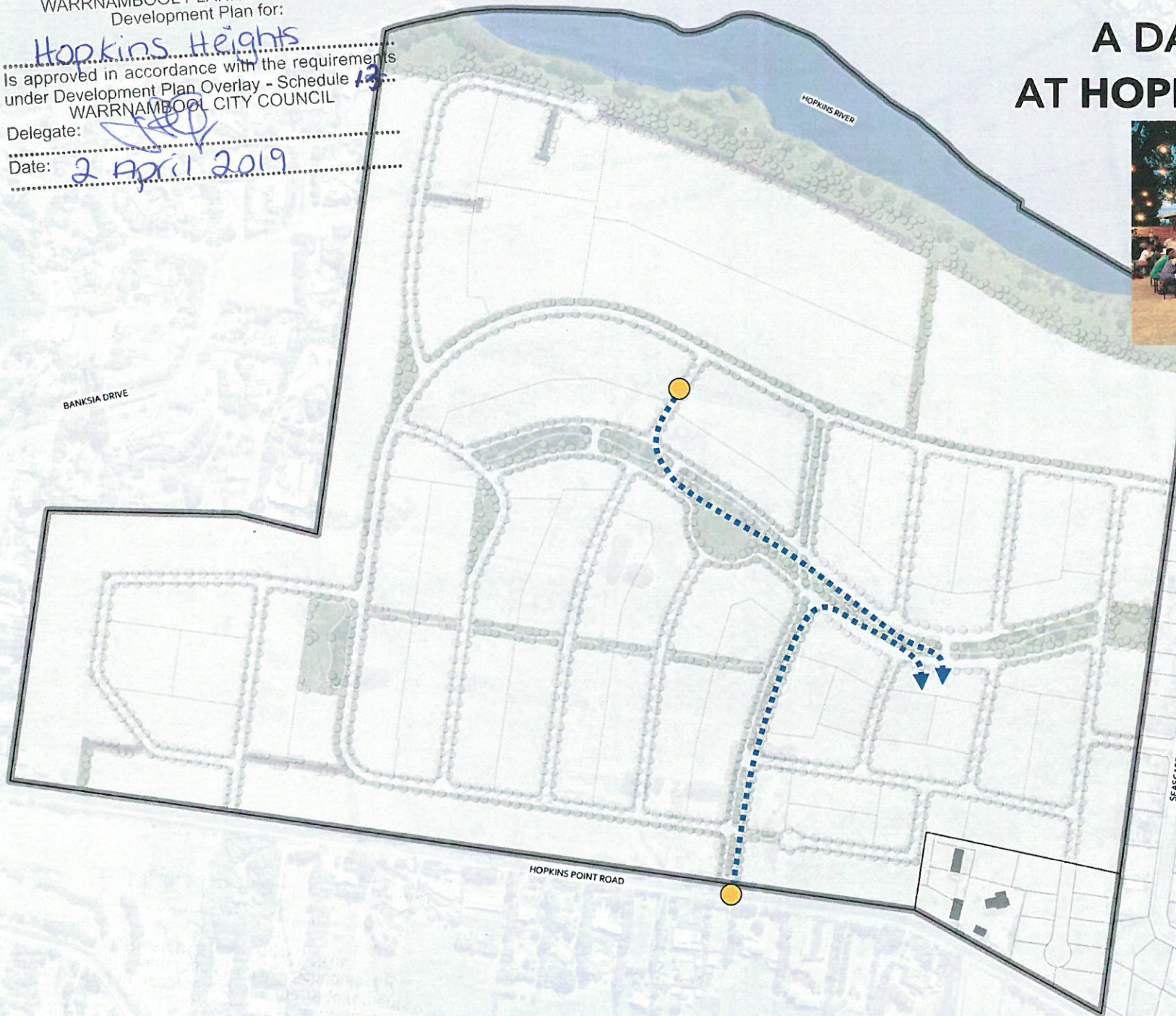
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A DAY IN THE LIFE AT HOPKINS HEIGHTS



7:30am
Morning jog or stroll with the family
dog through the neighbourhood
parks

8:30am
Walk to the bus with kids and catch
the bus to school. Cycle through the
linear park and into Warrnambool
city centre for work

11:30am
Walk to the 'village centre' shops for
fresh produce, complete daily tasks
and catch up with friends over coffee.
Walk across the central plaza and
enjoy brunch in the sun

3:45pm
Meet kids at the bus stop after school
and return home via park.
Cycle to local park and kick footy
with the kids

6:30pm
Return home from work
and meet family at local
restaurant for dinner

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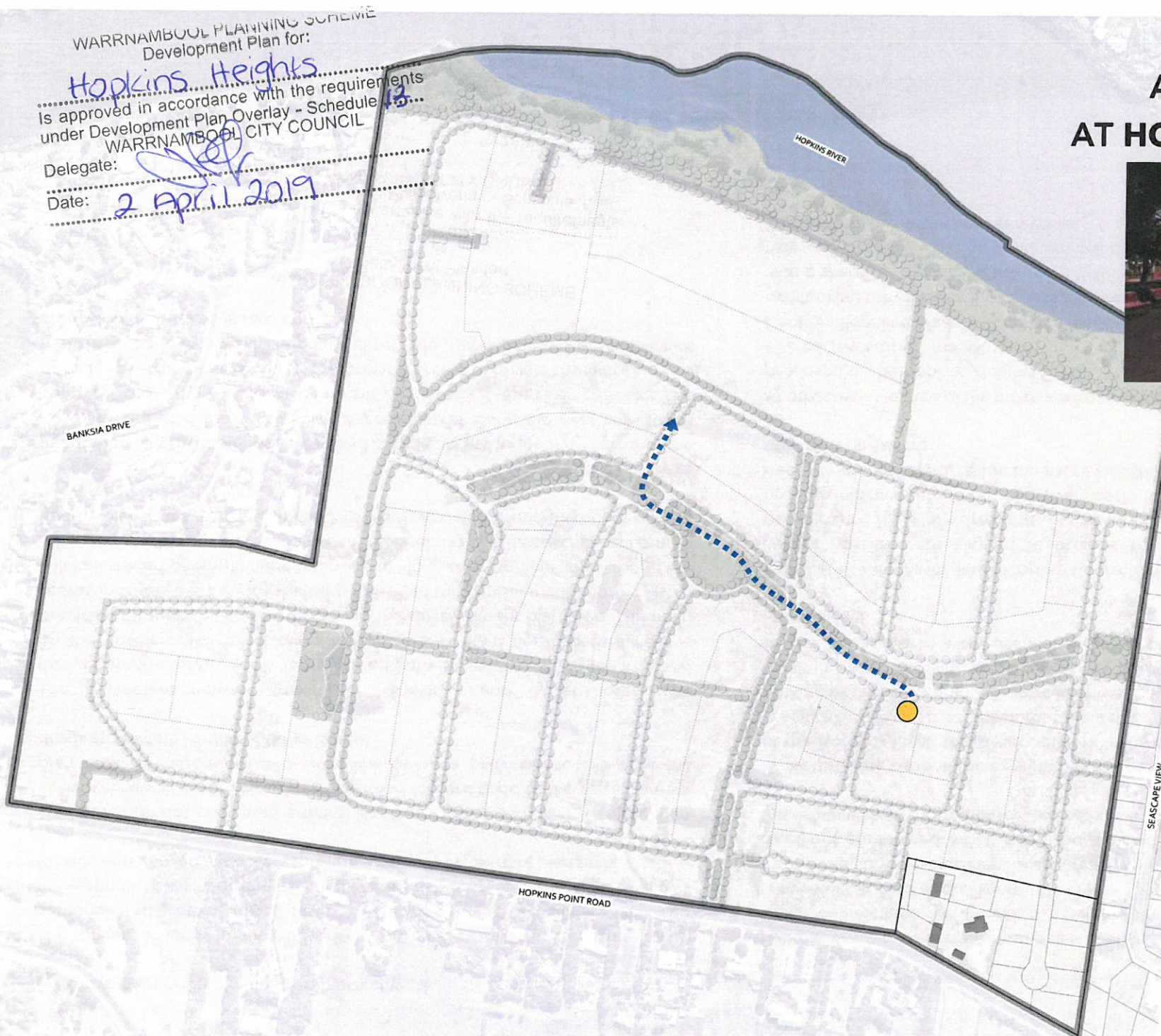
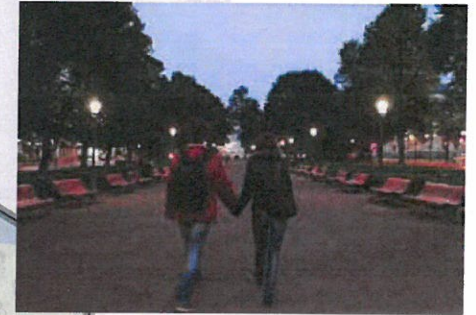
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A DAY IN THE LIFE AT HOPKINS HEIGHTS



7:30am
Morning jog or stroll with the family
dog through the neighbourhood
parks

8:30am
Walk to the bus with kids and catch
the bus to school
Cycle through the linear park and
into Warrnambool CBD for work

11:30am
Walk to the 'village centre' shops for
fresh produce, complete daily tasks
and catch up with friends over
coffee. Walk across the central plaza
and enjoy brunch in the sun

3:45pm
Meet kids at the bus stop after
school and return home via park.
Cycle to local park and kick footy
with the kids

6:30pm
Return home from work and meet
family at local restaurant for dinner

8:00pm
Walk home together

6.1 Movement network and road hierarchy

The proposed movement network is broadly consistent with the layout contemplated within the Coastal / Hopkins River Structure Plan. The development plan incorporates a highly permeable street network which allows for safe and convenient access throughout the development plan area.

The development plan area adjoins Hopkins Point Road to the south. Two vehicle access points to Hopkins Point Road are provided to the development plan area. An additional two road linkages are provided to the adjacent residential estate (Hopkins Ridge Estate).

The movement network maximises pedestrian and cyclist accessibility throughout the estate. A 2.5 metre shared path is provided along the frontage on the northern side of Hopkins Point Road. A main north-south access is provided through the central portion of the development plan area. The main access includes a 2.5 metre shared path within the central median providing a link to a shared path within the open space along the ridgeline and through the 'village centre'. The shared path continues along the 'Landscape Screening Road' which forms a cycling link back to the shared path along the Hopkins Road frontage.

In addition to providing pedestrian and bicycle access to the river corridor, the development plan provides vehicle access to and along the river corridor. A common property 'Access Place' is proposed along the river corridor. The primary role of the Access Place is to provide access to private dwelling. The 2.5 metre shared footpath / cycle path will provide pedestrian and bicycle access along 350 metres of the river corridor.

Footpaths are provided on both sides of all internal roads. A number of open space connections are provided between streets. The street grid provides convenient access and efficient movement to encourage walking and cycling throughout the development plan area. The street grid also ensures future dwellings are located within walking distance to future public transport within the estate with all properties located within 400 metres of the bus route.

While there are currently no existing bus routes which service the area to the east of the Hopkins River, the street network incorporates a bus route with streets specifically designed to accommodate buses (3.5 traffic lanes and on-street parking exclusively within 2.3 metre intended bays). The proposed bus route will result in buses travelling directly past the proposed 'village centre', providing convenient access to a key destination and access to residents of the Hopkins Ridge Estate.

Roads are aligned with natural topography and drainage lines within the area and (where possible) are aligned to provide best orientation for passive solar performance for future dwellings. Four-way intersections have been avoided in order to minimise infrastructure delivery costs. Roads straddling boundaries have been avoided where possible to ensure landowners can independently develop their own parcels.

Another major feature of the movement network and road hierarchy is the entrance proposed to the Hopkins Heights Estate. It incorporates a separated road within a large landscaped median strip that runs north to the Village Centre and the Central Ridgeline Park and to the Landscape Screening Road to the north overlooking the Hopkins River. This will create a dramatic and stunning entrance with a shared cycle / pedestrian path within the central median. While the road reserve is broad, the design treatment is such that it will calm traffic conditions and limit 'racing' behaviour between vistas.

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Great care has been taken in the initial design and will continue through detailed design to ensure wide roads incorporate traffic calming devices to limit traffic speed. In addition, landscaping at street entrances and within parking areas in the streets will be used to create greater intimacy and street softening through integration of vegetation within road reserves.

The Central Ridgeline Park also has a one-way road system on either side. This will reduce pavement width and traffic speed, making it a safer environment for pedestrians to use whilst improving access to the Central Ridgeline Park.

Guidelines for planning permit applications:

- Applications for subdivision must be accompanied with a Traffic Engineering Assessment which identifies existing traffic conditions, assesses suitability of proposed internal movement network and access arrangement and evaluates traffic impact on the surrounding road network.

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Figure 14: Movement network - Cross-sections plan

Cross-section A - Typical Access Place

The majority of streets within the development plan area are Typical Access Streets which consists of a 16 metre reservation which incorporates a 6.0 metre pavement, and a nature strip and footpath on both sides of the street.

The proposed Typical Access Place cross-section is set out in Figure 15.

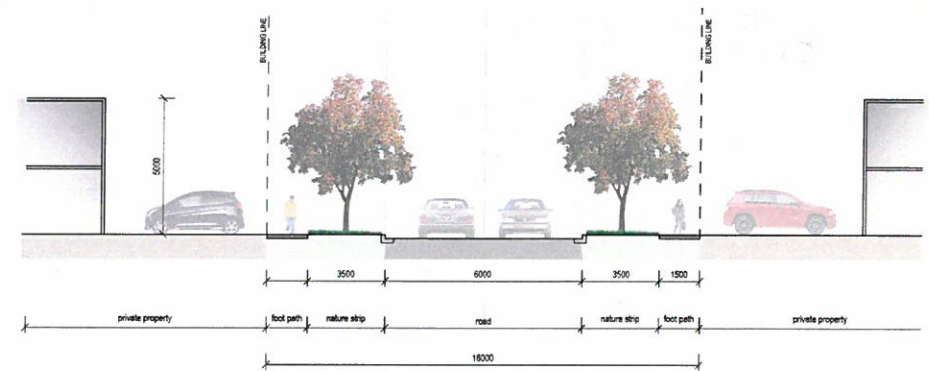


Figure 15: Typical Access Street

Cross-section B - Typical Access Street

The majority of streets within the development plan area are Typical Access Streets which consists of a 16 metre reservation which incorporates a 7.3 metre pavement, and a nature strip and footpath on both sides of the street.

The proposed Typical Access Street cross-section is set out in Figure 16.

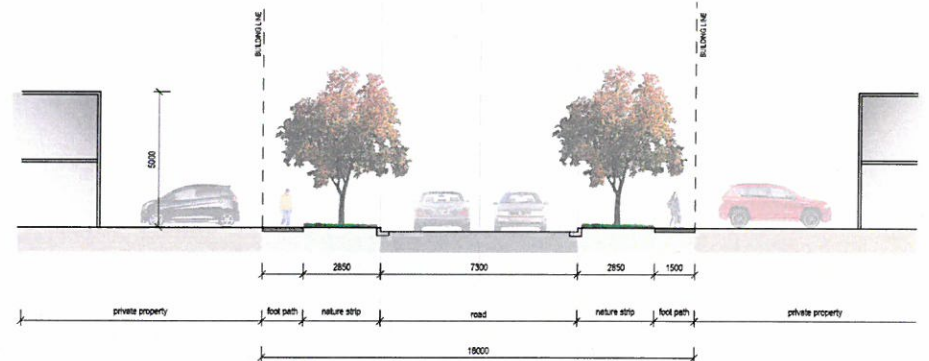


Figure 16: Typical Access Street

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Cross-section C - Wider Access Street

The structure plan envisaged a service road along the Hopkins Point Road frontage to provide access to properties along Hopkins Point Road and to provide for a residential frontage to the development area.

The development plan proposes an alternative design response which provides for residential development along Hopkins Point Road with provision for rear vehicle access. A wider street type is proposed for the Access Street which runs in an east-west direction along the southern portion of the development area. The road has been designed to accommodate a future bus route. Accordingly, the road has been designed with a wider 9.3 metre pavement incorporating 3.5 metre traffic lanes and an intended parking lane on the south side.

The Development Plan incorporates design guidelines to ensure the dwellings are designed to front Hopkins Point Road while respecting the character of the rear street.

The proposed Wider Access Street cross-section is set out in Figure 17.

Cross-section D - Landscape Screening Street with Bus Route

Landscape Screening Streets incorporate landscape screening within a wider verge on one side of the street. The street is proposed to coincide with the proposed bus route and will also incorporate a shared pathway within the widened verge. The Landscape Screening Street (with Bus Route) consists of a 22 metre reservation which incorporates a 11.6 metre pavement, comprising 3.5 metre traffic lanes and intended parking lanes on the both sides of the street, and 2.5 metre shared footpath on one side of the street.

The proposed Landscape Screening Street with Bus Route cross-section is set out in Figure 18.

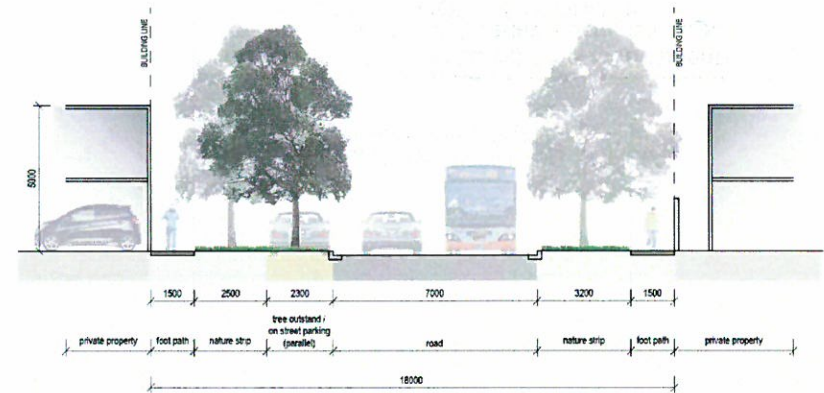


Figure 17: Wider Access Street

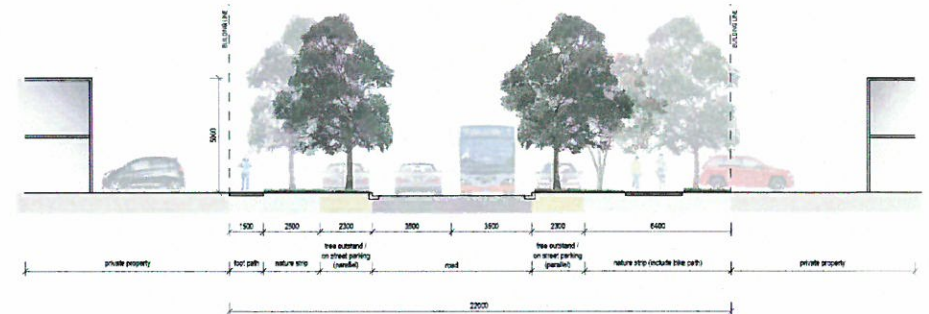


Figure 18: Landscape Screening Street with Bus Route

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Cross-section E - Landscape Screening Street without Bus Route

The Landscape Screening Street without Bus Route consists of a 20 metre reservation which incorporates a 7.3 metre pavement, and a nature strip and shared path / footpath on both sides of the street.

The proposed Landscape Screening Street without Bus Route cross-section is set out in Figure 19.

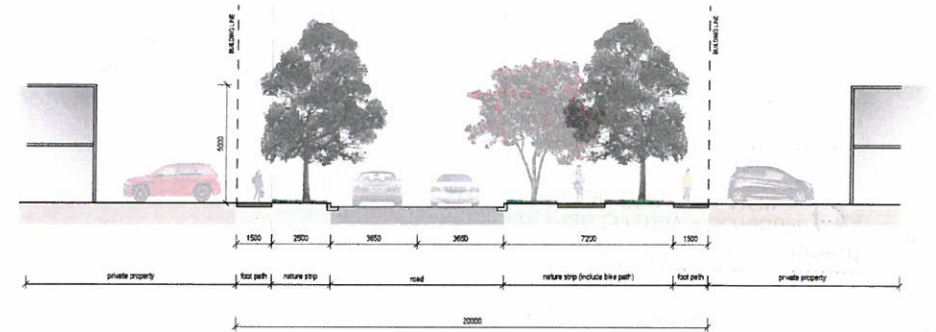


Figure 19: **Landscape Screening Street without Bus Route**

Cross-section F - Main Entrance Road

The main entrance road consists of divided carriageways with side of a 7.4 metre wide median within a 27 metre reservation. As the main entrance road contains a bus route, the carriageway incorporates a 3.5 metre traffic lane with intended parking lanes on both sides of the street.

A shared path is located within the median. With only a small number of median opening proposed along the route, this will provide a high standard facility for cyclists with minimal interaction with traffic and pedestrians. In addition to the shared path, footpaths are provided on both of street.

The proposed Main Entrance Road cross-section is set out in Figure 20.

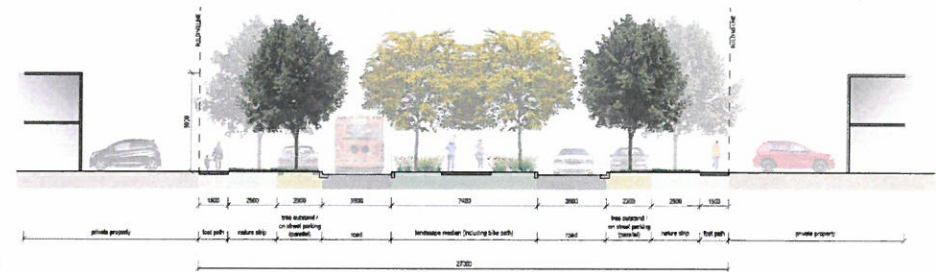


Figure 20: **Main Entrance Road**

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Cross-section G - Central East-West Street

A key feature of the development plan is the east-west street containing a wide open space central area which follows the ridge line. The Central East-West Street is proposed to be provided within a 45 metre reservation incorporating a landscaped central open space link and a shared bicycle path. The proposed cross-section differs along the length of the road, with 5.8 metre carriageways and intended parallel parking within the residential section of the street and wider 10.3 metre carriageways incorporating 45-degree angle parking through the mixed-use precinct ('Hopkins Heights village centre').

Cross-sections for the Central East-West Street are set out in Figures 21 and 22.

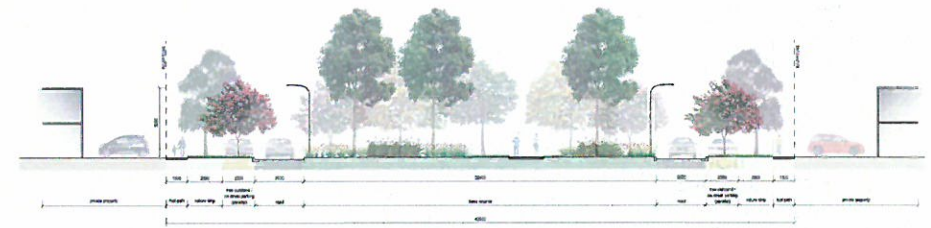


Figure 21: **Central East-West Street (Residential Section)**

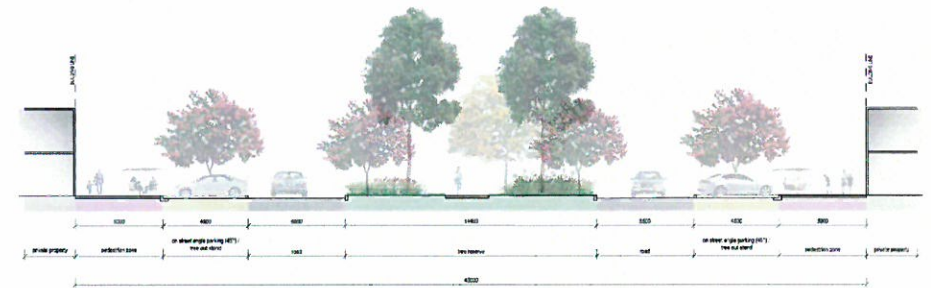


Figure 22: **Central East-West Street ('Village Centre' Section)**

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Private Roads

The proposed location of each of the Access Lanes is set out in Figure 14. All Access Lanes will be private owners corporation roads.

The use of Access Lanes has been minimised except where necessary due to restrictions associated with the topography of the site. In each case, the Access Lanes are proposed to be designed with a carriageway of 5.5 metres within a reservation of 10 metres to 12 metres.

Pedestrian / Bicycle Network

As set out in the above cross-sections, the development plan identifies a shared path network, both on and off road, linking linear spaces, planned open spaces and drainage reserves, resulting in a series of looped walking and cycling paths.

Pedestrian access to, and throughout the Hopkins Heights Estate has been carefully planned. The design of paths and the interconnection of paths will allow people to safely walk throughout the estate, enjoying the public spaces and views. The design creates high quality pedestrian permeability linking through adjoining and close by estates to the foreshore promenade and Logans Beach.

Bicycle access to and through Hopkins Heights Estate is of a very high quality with the provision of dedicated off-road cycling / pedestrian paths. Within the estate is an extensive bicycle path network, promoting safe bicycle use as a realistic alternative to motorised travel within the estate. Cyclists will also have access via

the shared paths to the wonderful public spaces, parks and reserves and to the foreshore promenade and Logans Beach.

Public transport

The development plan identifies a road network to accommodate a bus service for the neighbourhood and broader area.

Further to discussions with Public Transport Victoria, options for a potential bus route within the development plan area have been preserved through the design of road cross sections which provides for a bus route.

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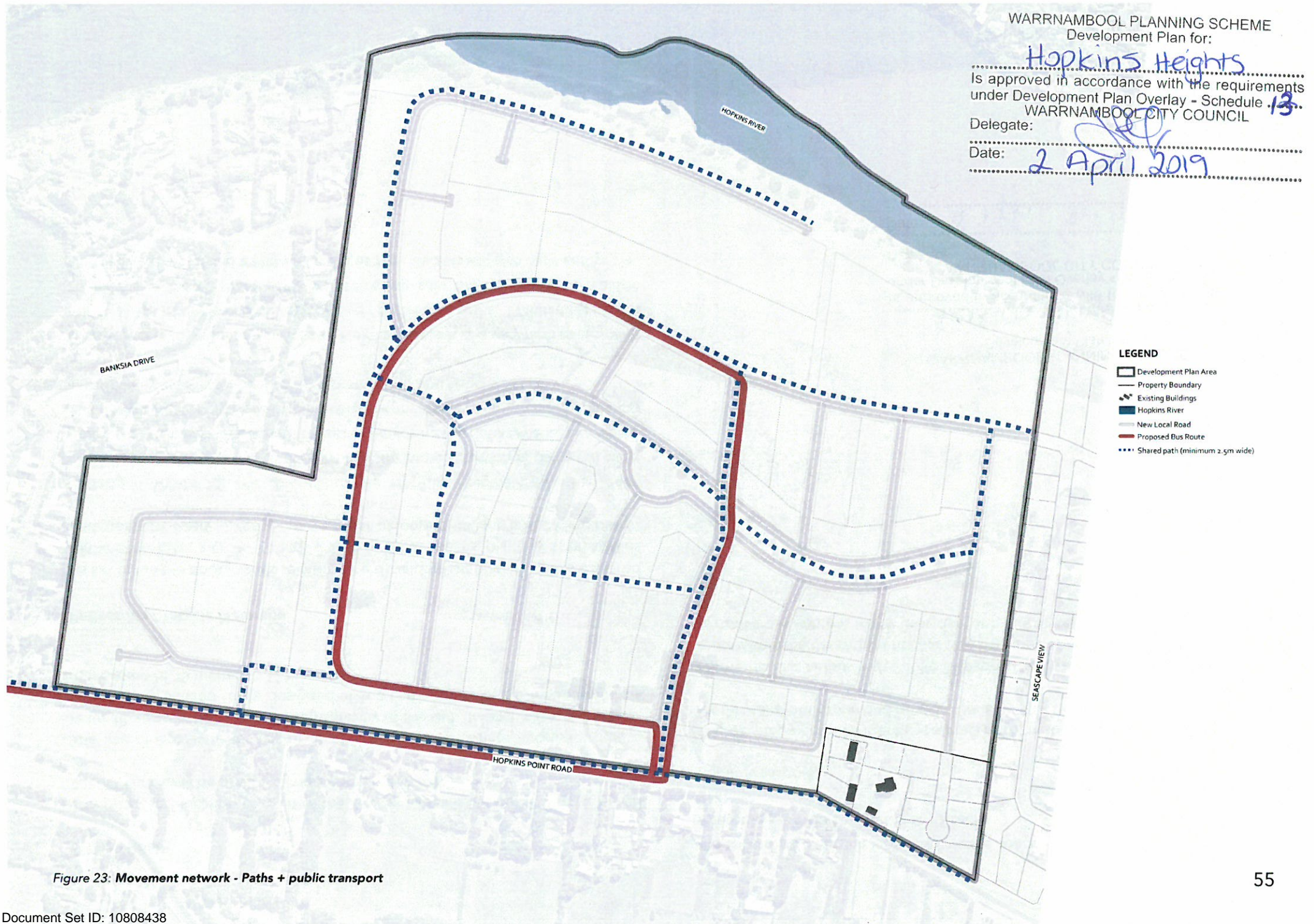


Figure 23: Movement network - Paths + public transport

6.2 Open space, landscape and natural systems

Open Space

The design of the open space network responds to the site conditions, drainage infrastructure requirements and open space requirements of the future community.

The Coastal / Hopkins Structure Plan encourages the provision of a range of open spaces including a series of small local parks, a biodiversity corridor along the Hopkins River, and a linear open space corridor along the ridgeline. Overall, the structure plan seeks to provide open space provision of approximately 6.5 hectares per 1000 people.

The schedule to the Development Plan Overlay encourages the provision of a single 1-hectare park located to the south of the ridgeline. Rather than providing open space within a single 1-hectare park, the Development Plan includes a series of open spaces to cater for a broad range of users and ensures all dwellings are within a safe walking distance of quality open space.

The open space network focuses on providing spaces which can easily adapt to different uses in response to changing community recreational preferences. In total, the Development Plan includes a network of public open space with a combined area of over 2.5 hectares (within a series of pause places, linking spaces, entrance median, and over-sized nature strips). These open spaces are in addition to the proposed river corridor, public spaces within the proposed 'village centre', and linear park along the ridgeline. The Development Plan includes open space provision in excess of 7 hectares per 1,000 residents.

While the open space network is intended to serve a variety of purposes, each open space area has been categorised according to its primary function as set out on the following pages.

Of particular significance is the Central Ridgeline Park, which runs along the highest ridge in Hopkins Heights Estate. It is a key component of the overall estate design. The Central Ridgeline Park spans east to west across the estate. It utilises existing vegetation that will provide an immediate green area, augmented and replaced over time with new vegetation species. The Park protects and enhances the use of elevated ridge providing great spaces for people to use and experience the dramatic views to the coast, the Hopkins River and the expansive northern hinterland.

Surrounding this central green spine will be housing and a central mixed-use 'village centre'. The village centre is a key component in creating a focal point for community meeting and engagement. Hopkins Heights Estate is not 'just a housing estate'. The village centre provides a vital heart to the estate where residents can meet, greet and congregate enjoying a sense of place and true community. The village centre includes commercial uses that are permitted in the residential zone. Home businesses and offices will be encouraged. The village centre will have a purposeful intimate feel through building design and location, and public space design and landscape. Housing densities around the Central Ridgeline Park will be higher than surrounding areas to encourage activity, surveillance, safety, intimacy and to provide different housing options for people seeking smaller, more manageable lots. Public transport will collect and drop people at the village centre. It will be a great place for people of all ages, backgrounds and household composition.

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Landscaping

Significant landscaping of road reserves, public areas and private land is critical to achieving the vision for an aesthetically attractive, community focussed estate. The objective is to gradually immerse the built form with vegetation using a combination of planting on public land, road reserves and private lots.

The central east west ridge line has significant vegetation planted over thirty years ago. This vegetation initially will be retained and incorporated in the Central Ridgeline Park. The height of the vegetation and topography of the adjacent land will ensure houses around the Central Ridgeline Park are lower than the height of the existing vegetation.

Road reserves are of sufficient width to accommodate significant tree planting in nature strips. The open space areas are designed and located to incorporate extensive vegetation which will enhance the local environment and be visible from outside the estate.

The western and northern sections of the bus route have 22-metre-wide road reserves which provide wider areas for trees and vegetation in the road reserve. This will provide sight screening from external viewing points to the west and north of the estate.

The main entrance median strip provides a great space for additional tree planting and vegetation along this south to north spine.

All house lots should have least three canopy trees planted within the lot. More exposed sites on steeper slopes will incorporate landscape envelopes with specific landscape requirements.

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Figure 24: Open Space Network

Entrance

The entrance into the development plan area from Hopkins Point Road will be enhanced with an avenue of canopy trees to both sides of the nature strip as well as the median, along with low understory planting to provide a sense of pleasure within the area for both vehicular and pedestrian traffic. The 7.4 metre wide central median includes a shared path which provides a safe and enjoyable access to the 'Hopkins Heights village centre' from the main road. In addition, a feature gateway element design will be explored to create a sense of arrival and context. This will include material (such as timber, bluestone and weathered steel) which represents the coastal character of the area.

To the northeast of the entrance is a typical landscape treatment of a 'Linking Space', which provides shared pedestrian and bicycle access.

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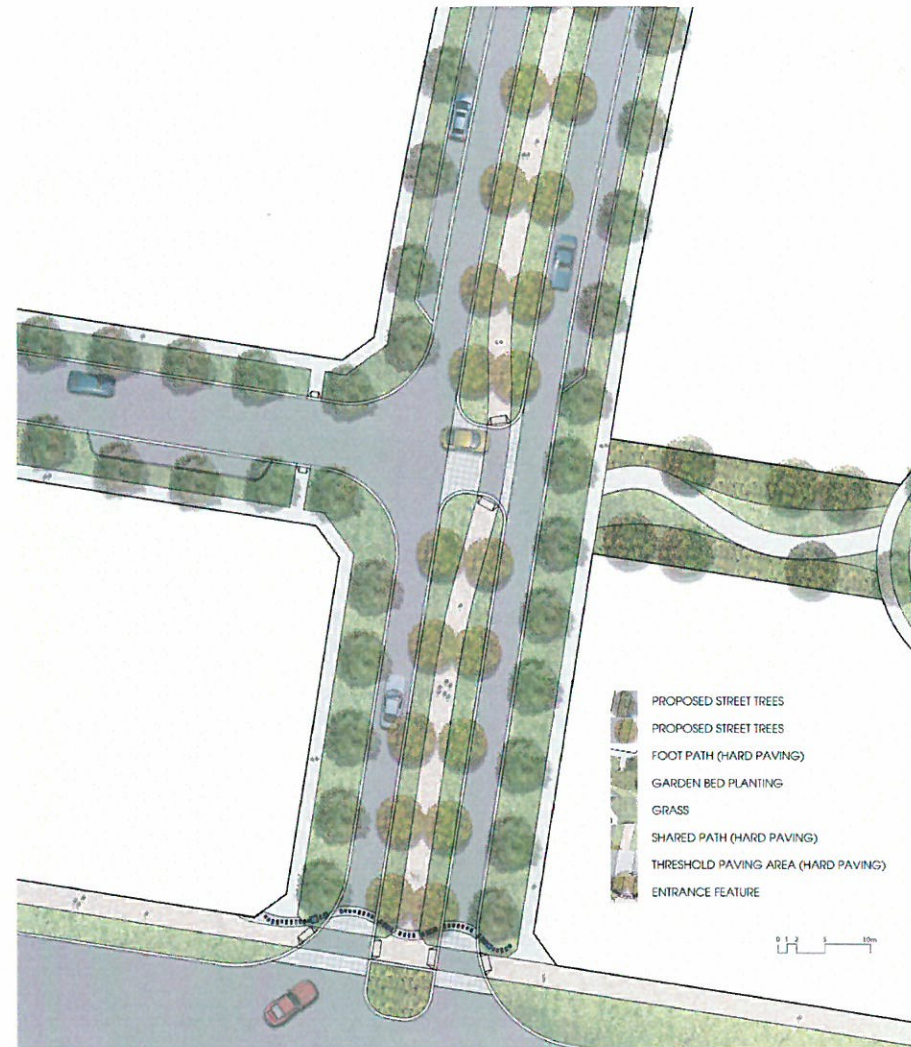


Figure 25: Entrance

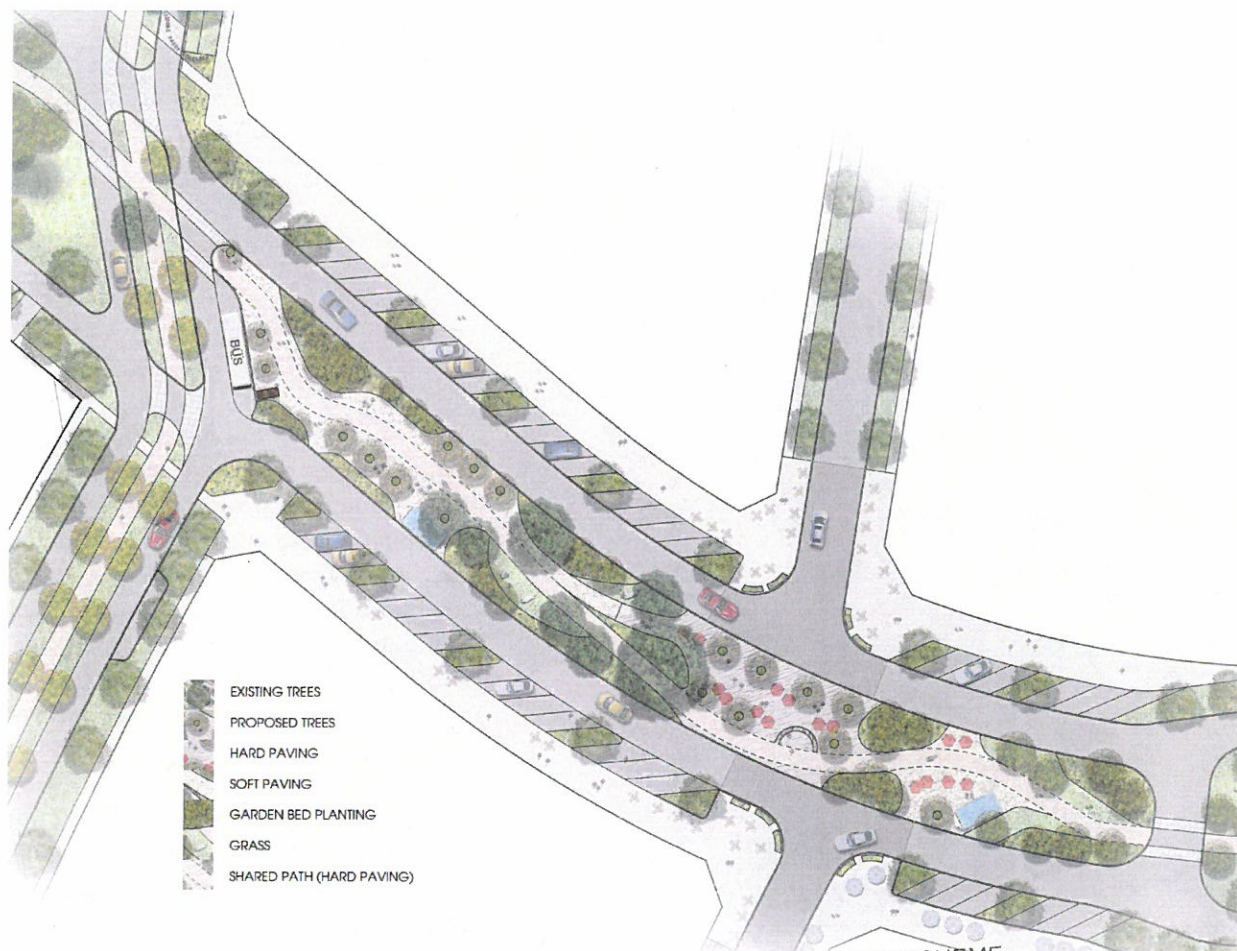


Figure 26: Hopkins Heights 'village centre'

Hopkins Heights 'village centre' is a linear park made up of three distinct areas along the Central Ridgeline Park, located right in the centre of the Hopkins Heights 'village centre' (local shops, businesses, restaurants and cafes). This linear park serves an important role in the connectiveness of the local community, as it provides a range of flexible spaces that cater for different activities.

These spaces are defined as areas including:

- An urban area characterised by a hard-paved ground surface, trees groups and planting buffers with seating walls. This area will be designed with opportunity for food trucks and temporary market stalls in mind.
- A semi-urban area located at the centre of the 'village centre' defined by a mix of hard paving, low garden beds and trees (including large existing trees). The road surrounding this space will be raised to be flush, providing an easy transition for pedestrians and cafe patrons, while reducing the speed of vehicular traffic, ensuring the safety of the users of this public open space.
- A 'bushland' area characterised by softer paving, such as granitic sand or mulch, mass planting of native grasses and trees planted throughout at distances you can walk under. This area is designed to evoke a feeling walking/cycling through a forest of trees or bushland.

Whilst these spaces are seemingly different, they are connected through the shared path which passes through (connecting the east and west) the areas. Planting within the Central Ridgeline Park will predominantly be indigenous species, with large existing trees providing protection to the new planting as nursery.

Central Reserve

Located within the Central Ridgeline Park, the Central Reserve is a focal point to the Hopkins Heights Estate, and provides a range of open space amenity to a larger catchment of residents and visitors. The reserve is located at a primary vantage point with long distance views to Port Fairy across the Warrnambool Breakwater.

The reserve includes a central kickabout oval that can be used for informal play or as a location for community gatherings/events; and a playground with both structured and imaginative play elements. As part of the central landscape ridge, it also provides connections via the share path which links the east to the west of the development site.



Figure 27: **Central Reserve**

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Figure 28: Local Reserve

Local Reserve

The local reserve is located at a primary vantage point with spectacular views of the Hopkins River mouth.

The location also takes advantage of some large existing Cypress trees that will create a park structure until such time other planted trees can mature.

The design of this reserve takes into consideration the proximity of neighbouring communities to public open space and provides an opportunity to include two informal kickabout ovals, a basketball half court, as well as a shelter with picnic settings and park benches.

View corridors are considered to ensure the magnificent vistas out to the Hopkins River mouth are available to park users.

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Pause Place 01

This Roadside Reserve provides a rest area which takes the form of a small 'outdoor room' set within a park-like environment. The widened area along the share path provide ideal opportunities for the creation of local open spaces that can be used for circulation, meeting place, social activities and passive recreation. In addition, it provides visual and landscape amenity to adjoining residences. The combined usage of low gabion walls and landscape planting enhances the experience of the users by creating a gateway for cyclists and pedestrians.



Figure 29: Streetscape Pause Place 01

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Pause Place 02

This Roadside Reserve provides a small rest area which takes the form of a semi-circular 'outdoor room' set within a park-like environment. The widened area buffering the outdoor room from the road provides a safer environment for the usage of this space as a meeting place between friends and neighbours. In addition, it provides visual and landscape amenity to adjoining residences. The proposed tree planting to this reserve accentuates the openness of the space, while providing shade and enhances the user experience by creating a comfortable resting spot, at the same time highlighting the view corridor towards the coast.

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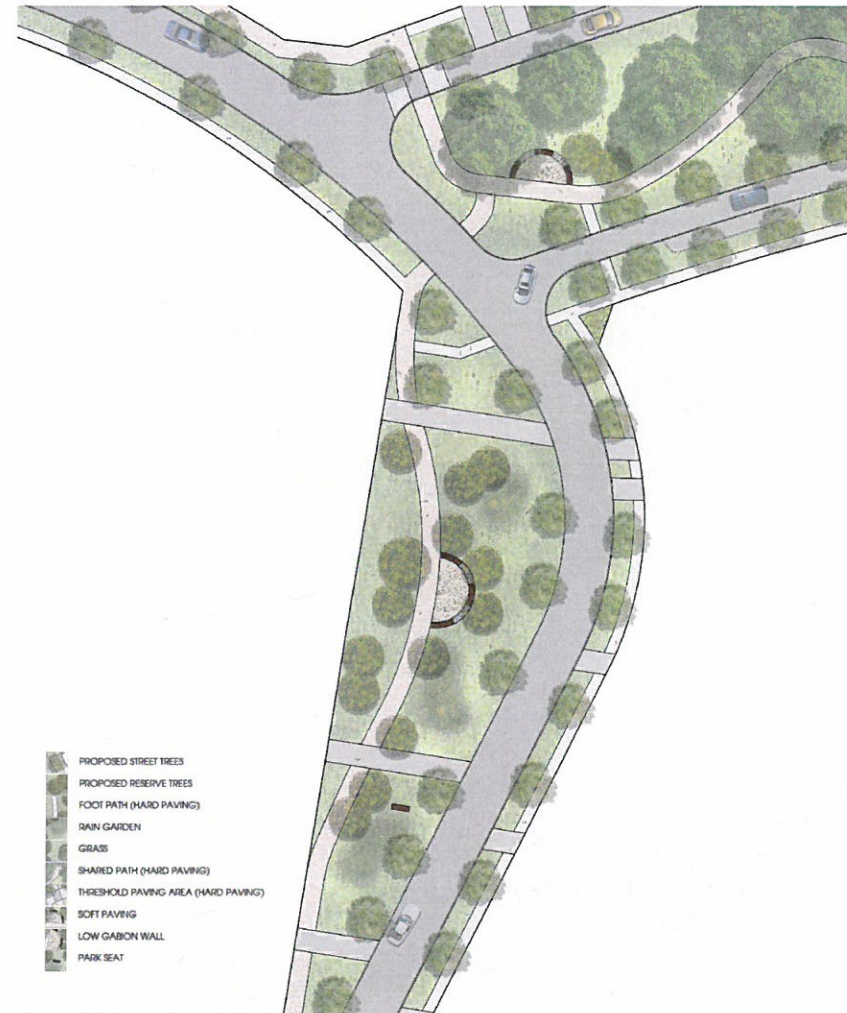


Figure 30: Streetscape Pause Place 02

Linking Space

A typical linking space which provide local links and connections between streets and open space reserves. They provide alternative routes between neighbourhood pockets and can delight and surprise with well landscaped edges and spaces.

A generous 2.5m wide shared path meanders through the linking space, flanked by a grassed lawn, offers unimpeded views towards the ends of the landscape corridor. Layered landscape planting including small trees to the sides of the linking space helps in framing the space and softens the side fencing to the adjacent lots.

River reserve

A 4-hectare river corridor parkland is provided along the banks of the Hopkins River. Extensive revegetation within the river parkland corridor will reinforce the character and ecological values of the Hopkins River. A combination of hard and soft landscape treatments including shared paths, seating, shade trees and fishing facilities will support the parkland's passive recreation role and reinforce the natural character of the reserve.

Detailed elements such as revegetation, shade tree planting, park furniture (for example seating), bollards, materials, path connections and fishing facilities will be addressed as part of a detailed masterplan for the river corridor.

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Figure 31: Typical Linking Spaces

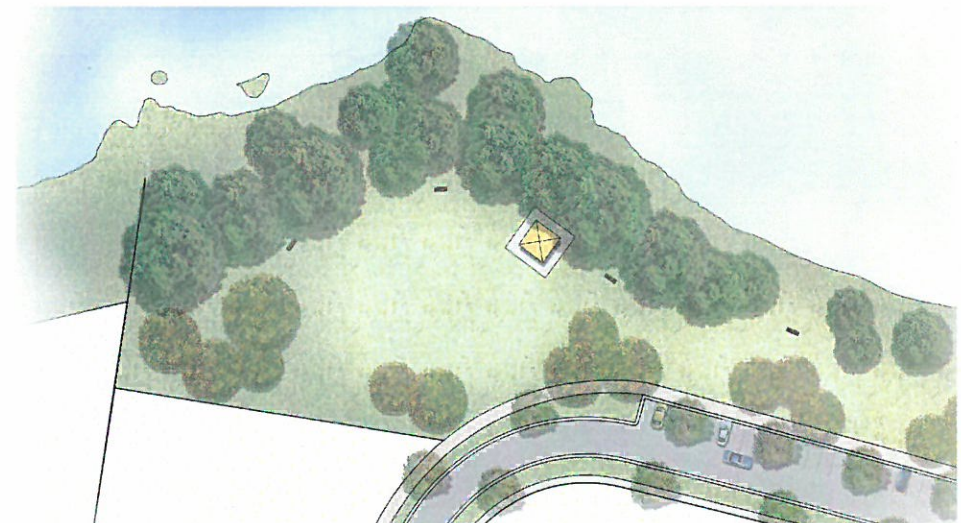


Figure 32: River Reserve



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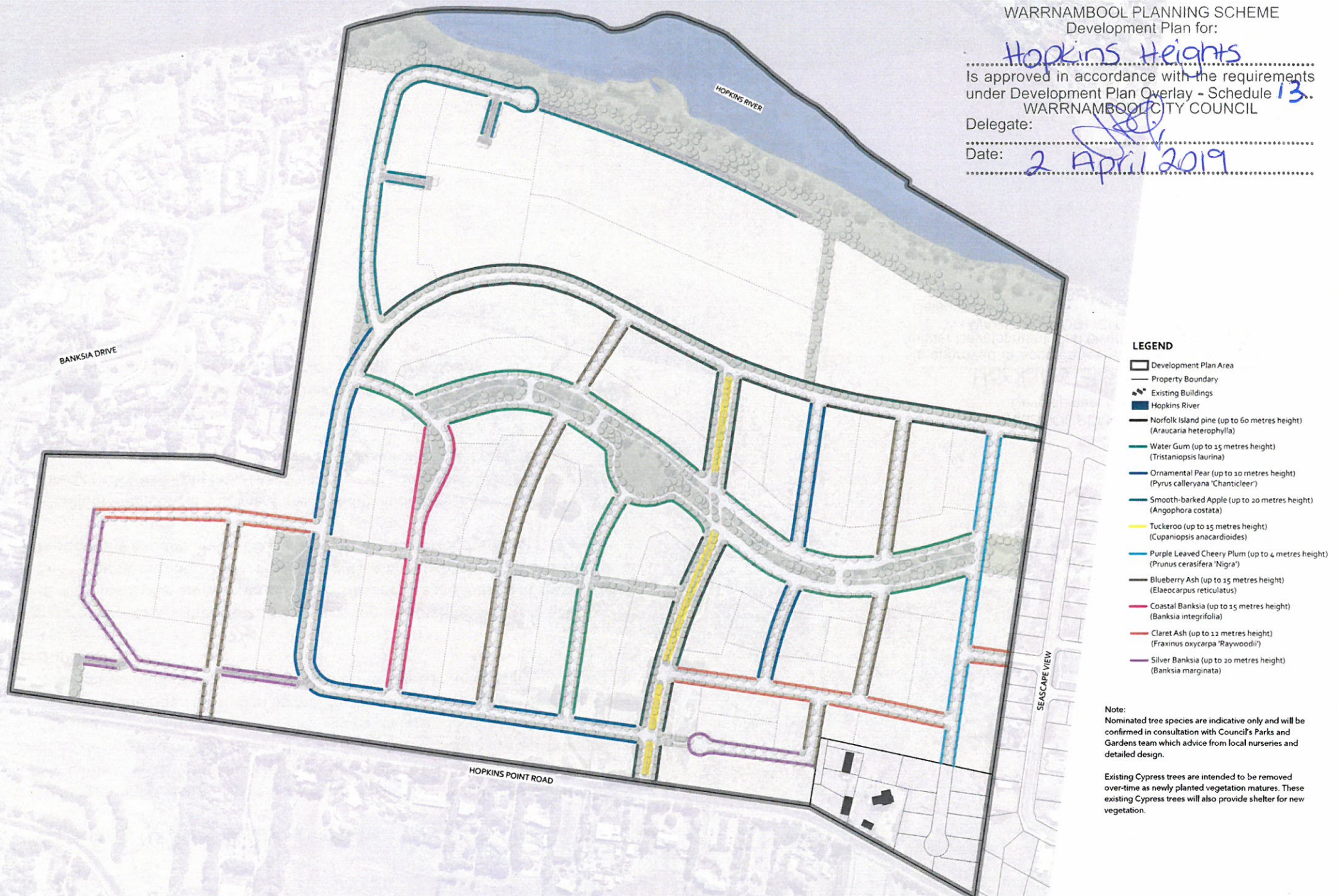


Figure 33: Street tree themes

6.3 Housing, Neighbourhoods and Character

Neighbourhood densities are broadly consistent with densities contemplated within the Coastal / Hopkins River Structure Plan. Neighbourhood densities have been designated based on topographical constraints, drainage catchments and proximity to open space and the 'village centre'. Conventional residential densities have been located in flatter / less steep parts of the development plan area. Larger allotments have been located in steeper areas (such as along the Hopkins River).

Figure 33 sets out the preferred subdivision and neighbourhood design. The development plan area has three distinct precinct areas (Coastal, ridge and river). Each neighbourhood will require a different approach which responds to topography and the preferred character of the neighbourhood.

All allotments contain sufficient area for substantial revegetation (at least two-three canopy trees per lot). The street network and lot layout has been designed to facilitate change overtime in response to evolving social preferences.

Within these precincts there are 'seven' character areas. Each character area will have its own suite of design guidelines. On the following pages is a summary of the indicative design guidelines which will be fully fleshed out at the time of detailed design.

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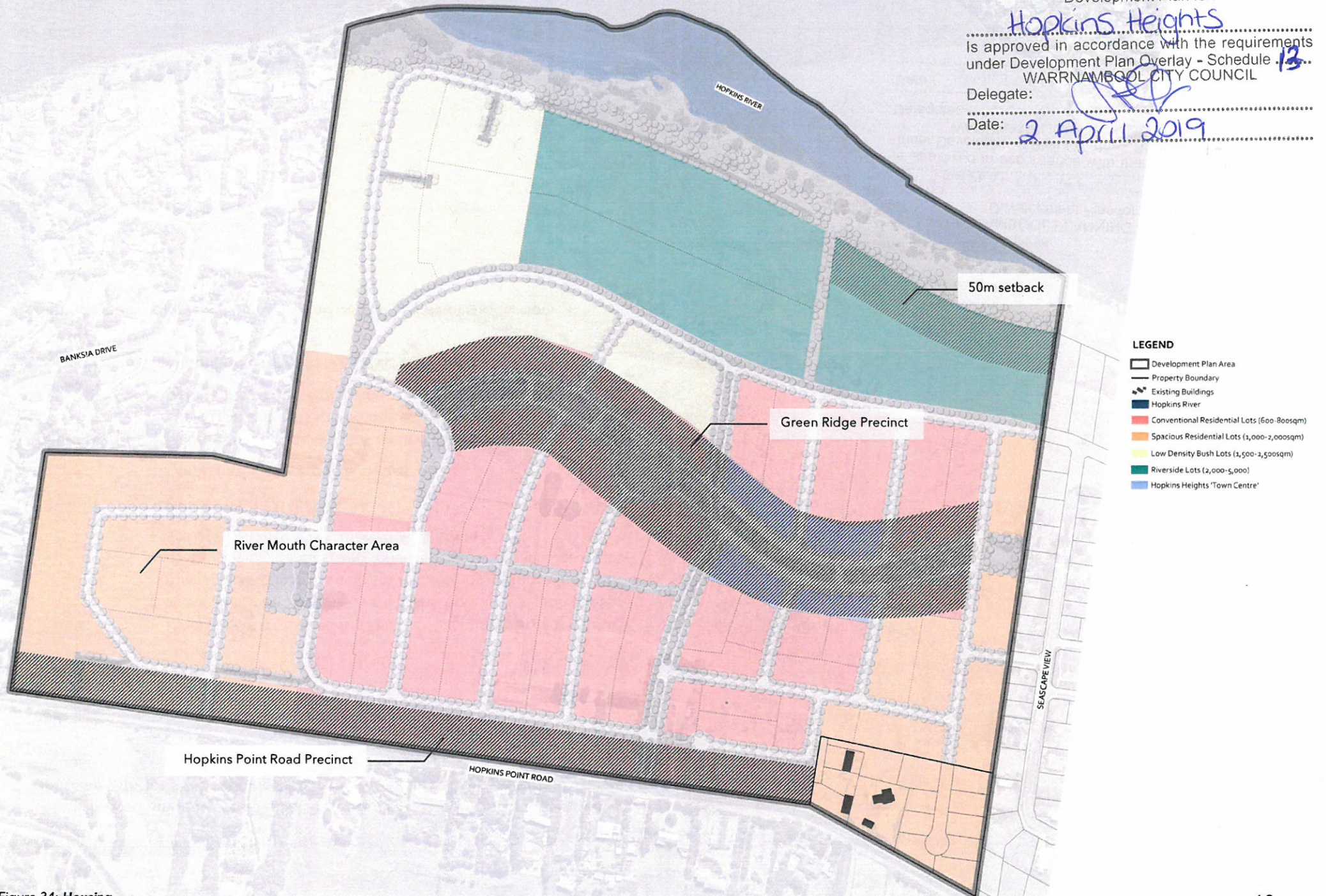


Figure 34: Housing

Conventional Residential Lots

Lots will accommodate residential development at conventional residential densities with lot sizes ranging between 600 square metres and 800 square metres.

Setbacks

The following setbacks apply:

- | | |
|---------------------|--|
| Front | Dwellings to be setback no less than 5.0 metres from the front street alignment. Porches, verandahs, masonry chimneys, etc. may encroach into the setback distance by no more than 1.0 metre. Eaves, fascias and gutters may encroach into the setback distance by no more than 600mm. |
| Rear | No less than 3.0 metres. |
| Side | Combined setback of 2.0 metres. |
| Garages | Garages should be setback no less than 6.0 metres from the street frontage and should be setback no less than 1.0 metre behind the front building line. |
| Outbuildings | Outbuildings should not be visible from the public realm. |

Setback requirements will be registered on title at time of subdivision.

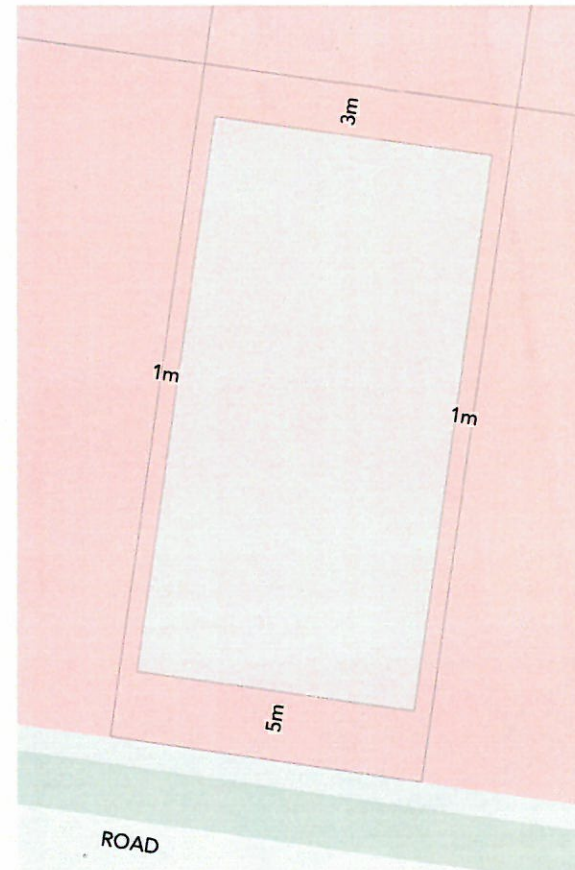


Figure 35: Conventional Residential Lots Building Envelope

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Character

Dwelling facades should include features such as porticos or verandahs to visually break up long walls. Garages should not dominate the appearance of the dwelling and must be a complementary and recessive element to the dwelling facade.

Designs for corner allotments should address both street alignments and/or linking space. Side elevations should be given equal attention to create 'multi-sided' buildings. The design should ensure habitable room windows overlook the side street and/or linking space.

Materials and colours

Dwellings should incorporate materials which respond to local climatic coastal conditions and should incorporate at least two contrasting materials / finishes to facades to ensure a visually interesting design and enhance streetscape appeal.

External building colours should reflect muted earthy tones which will blend with the coastal setting. A sample of colours for use on building exteriors is provided on pages 93-95.

Fencing

Front fence	Maximum height of 1.5 metres with minimum 50% transparency. Solid fencing below 1.2 metres in height is permitted.
Wing fence	Side wing fences must be setback at least 0.5 metres from the building facade and must be minimum 50% transparency. Solid fencing is not allowed.
Side fence	Solid fencing (greater than 1.5 metres in height) to a side street alignment should be setback 800mm to provide a landscaping strip along the street alignment. Fencing should have a minimum 50% transparency.

Fencing requirements will be registered on title at time of subdivision.

Access and driveways

Driveways should not dominate front gardens. They should be a secondary element to landscaping and not detract from the appearance of the streetscape.

The design and finish of driveways should complement the design of buildings and landscaping. A sample of materials and finishes is provided on pages 91-93.

Maximum width of crossover must be no wider than 5 metres.

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Garages

Garages and garage doors should complement the design of the dwelling and proposed landscape and be finished with similar or complementary materials.

Garage doors should be of a panel lift door or slim line sectional door variety. Roller doors are not permitted to the facade.

Garage width should not exceed the width of the main dwelling facade.

Landscape

At least three (3) canopy trees should be provided (at least one in the front garden and at least two in the rear garden). Canopy trees must include indigenous species suited to the coastal setting.

Every dwelling must provide for on-site rainwater collection in water tanks for garden irrigation.

Ancillary structures and service equipment

Ancillary structures and elements should be located so that are not readily visible from the public realm. This includes items such as:

- Hot water systems (including solar heating)
- Heating and cooling units
- Water storage tanks
- Satellite dishes and TV aerials
- Solar panels and batteries
- Washing lines.

If service equipment is roof mounted (such as heating and cooling units), these should be positioned below the ridgeline and coloured to match the roof as far as practical.

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Conventional Residential Lots (Green Ridge Precinct)

Similar to allotments within the 'Conventional Residential' area, allotments adjacent to the Green Ridge Precinct will accommodate medium density residential development, in some cases with zero setback to the street. Residential densities range between 600 square metres and 800 square metres.

Setbacks

The following setbacks apply:

Front	Dwellings to be setback a minimum of 4.0 metres from the front street alignment. <u>Sections of the dwelling may be permitted to be built to the street if the purpose is for a non-residential use permitted under the General Residential Zone (such as home occupation).</u> A wall on a front boundary should be limited to a total length of not more than 7.0 metres on that boundary
Rear	No less than 3.0 metres.
Side	Combined setback of 2.0 metres.
Garages	Garages should be setback no less than 5.0 metres from the street frontage and should be setback no less than 1.0 metre behind the front building line.
Outbuildings	Outbuildings should not be visible from the public realm.

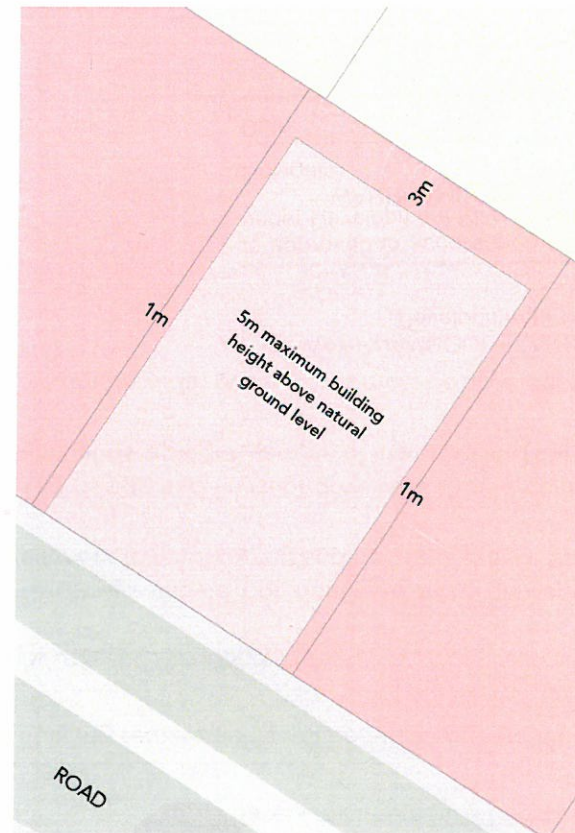


Figure 36: Conventional Residential Lots Building Envelope (Green Ridge)

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Maximum building height

Allotments within the Green Ridge Precinct have a maximum building height of 5.0 metres above natural ground level.

Building envelope, setback and height requirements will be registered on title at time of subdivision.

Character

Dwelling facades should include features such as porticos or verandahs to visually break up long walls. Garages should not dominate the appearance of the dwelling and must be a complementary and recessive element to the dwelling facade.

Designs for corner allotments should address both street alignments and/or linking space. Side elevations should be given equal attention to create 'multi-sided' buildings. The design should ensure habitable room windows overlook the side street and/or linking space.

Materials and colours

Dwellings should incorporate materials which respond to local climatic coastal conditions and should incorporate at least two contrasting materials / finishes to facades to ensure a visually interesting design and enhance streetscape appeal.

External building colours should reflect muted earthy tones which will blend with the coastal setting. A sample of colours for use on building exteriors is provided on pages 93-95.

Fencing

Front fence

Maximum height of 1.5 metres with minimum 50% transparency. Solid fencing below 1.2 metres in height is permitted.

Wing fence

Side wing fences must be setback at least 0.5 metres from the building facade and must be minimum 50% transparency. Solid fencing is not allowed.

Side fence

Fencing (greater than 1.5 metres in height) to a side street alignment should be setback 800mm to provide a landscaping strip along the street alignment. Fencing should have a minimum 50% transparency.

Fencing requirements will be registered on title at time of subdivision.

Access and driveways

Driveways should not dominate front gardens. They should be a secondary element to landscaping and not detract from the appearance of the streetscape.

The design and finish of driveways should complement the design of buildings and landscaping. A sample of materials and finishes is provided on pages 91-93.

Maximum width of crossover must be no wider than 5 metres.

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Garages

Garages and garage doors should complement the design of the dwelling and proposed landscape and be finished with similar or complementary materials.

Garage doors should be of a panel lift door or slim line sectional door variety. Roller doors are not permitted to the facade.

Garage width should not exceed the width of the main dwelling facade.

Landscape

At least three (3) canopy trees should be provided (at least one in the front garden and at least two in the rear garden). Canopy trees must include indigenous species suited to the coastal setting.

Every dwelling must provide for on-site rainwater collection in water tanks for garden irrigation.

Ancillary structures and service equipment

Ancillary structures and elements should be located so that are not readily visible from the public realm. This includes items such as:

- Hot water systems (including solar heating)
- Heating and cooling units
- Water storage tanks
- Satellite dishes and TV aerials
- Solar panels and batteries
- Washing lines.

If service equipment is roof mounted (such as heating and cooling units), these should be positioned below the ridgeline and coloured to match the roof as far as practical.

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Conventional Residential Lots (Hopkins Point Road Precinct)

Allotments along Hopkins Point Road are designed to ensure dwellings address Hopkins Point Road. Residential densities range between 600 square metres and 800 square metres.

Setbacks

The following setbacks apply:

Front	Houses must be designed to have frontage to Hopkins Point Road. Dwellings to be setback no less than 5.0 metres from Hopkins Point Road. Porches, verandahs, masonry chimneys, etc. may encroach into the setback distance by no more than 1.0 metre. Eaves, fascias and gutters may encroach into the setback distance by no more than 600mm.
Rear	No less than 3.0 metres.
Side	Combined setback of 2.0 metres.
Garages	Garages may encroach into the rear setback distance. Garages oriented to the rear road may be built on the building line of this road if the garage doors are at 90 degrees from the road alignment (see precedent images on page 76). A wall on a rear boundary should must be limited to a total length of not more than 7.0 metres on that boundary.
Outbuildings	Outbuildings should be screened from the public realm.

Building envelope and setback requirements will be registered on title at time of subdivision.

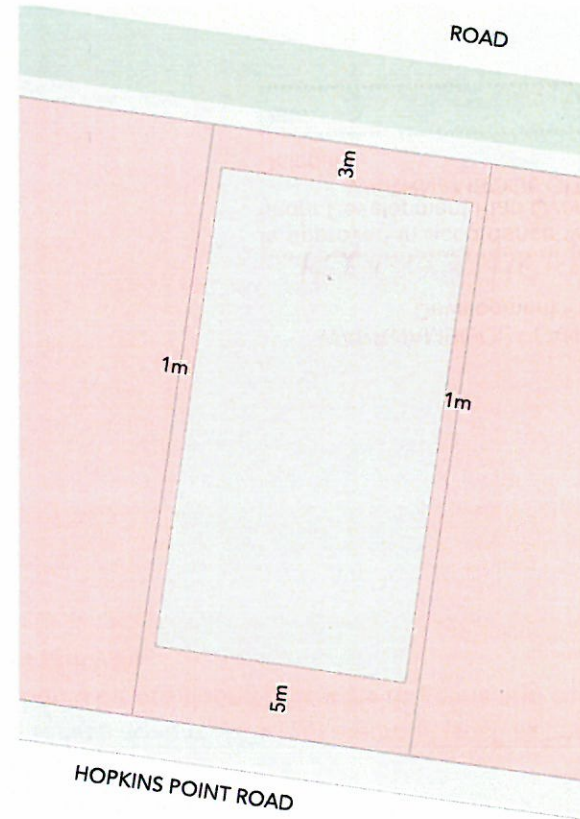


Figure 37: **Conventional Residential Lots Building Envelope (Hopkins Point Road)**

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Maximum building height

Hopkins Point Road Precinct **Area A** - Maximum building height of 7.5 metres above natural ground level. Where possible, building envelopes will be provided to enable view sharing of long-distance views to the Warrnambool Breakwater and Port Fairy.

Hopkins Point Road Precinct **Area B** - Maximum building height of 5.5 metres above natural ground level. Height will be limited to single storey to reduce potential visual impact of building form near the Hopkins Point Road cutting. Building envelopes will be provided to enable view sharing of long-distance views to the Warrnambool Breakwater and Port Fairy.

Hopkins Point Road Precinct **Area C** - Maximum building height of 7.5 metres above natural ground level reducing to single storey closer to the rear road frontage. Land within Area C contains slope which lends itself to split-level dwellings. Built form will be limited to single storey at the rear road frontage with opportunities to develop higher built form in response to slope.

Building envelope, setback and height requirements will be registered on title at time of subdivision.



Figure 38: Building Height Areas - Hopkins Point Road

Character

Dwelling facades should include features such as porticos or verandahs to visually break up long walls. Garages should not dominate the appearance of the dwelling and must be a complementary and recessive element to the dwelling.

Designs for corner allotments should address both street alignments and/or linking space. Side elevations should be given equal attention to create 'multi-sided' buildings. The design should ensure habitable room windows overlook the side street and/or linking space.

Materials and colours

Dwellings should incorporate materials which respond to local climatic coastal conditions and should incorporate at least two contrasting materials / finishes to facades to ensure a visually interesting design and enhance streetscape appeal.

External building colours should reflect muted earthy tones which will blend with the coastal setting. A sample of colours for use on building exteriors is provided on pages 93-95.

Fencing

Front fence

Maximum height of 1.5 metres with minimum 50% transparency. Solid fencing below 1.2 metres in height is permitted.

Wing fence

Side wing fences must be setback at least 0.5 metres from the building facade and must contain minimum 50% transparency. Solid fencing is not allowed.

Rear fences

Fencing along the rear street alignment should be:

- Setback at least 400mm from the street alignment to provide a landscaping strip, and
- No greater than 1.8 metres in height with minimum 50% transparency.

Planting of hedges or landscaping along street alignments is encouraged.

Side fences

Fencing (greater than 1.5 metres in height) to a side street alignment should be setback 800mm to provide a landscaping strip along the street alignment. Fencing should have a minimum 50% transparency.

Fencing requirements will be registered on title at time of subdivision.

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Access and driveways

Driveways should be a secondary element to landscaping and not detract from the appearance of the streetscape.

The design and finish of driveways should complement the design of buildings and landscaping. A sample of materials and finishes is provided on pages 91-93.

Maximum width of crossover must be no wider than 5 metres.

Garages

Garages and garage doors should complement the design of the dwelling and proposed landscape and be finished with similar or complementary materials.

Garage doors should be of a panel lift door or slim line sectional door variety. Roller doors are not permitted to the facade.



Precedent image: example of rear garaging
Photo credit: David Dreadon, GroupGS



Precedent image: example of rear feature fence
Photo credit: David Dreadon, GroupGSA

Landscape

At least three (3) canopy trees should be provided (at least one in the front garden and at least two in the rear garden). Canopy trees must include indigenous species suited to the coastal setting.

Every dwelling must provide for on-site rainwater collection in water tanks for garden irrigation.

Ancillary structures and service equipment

Ancillary structures and elements should be located so that are not readily visible from the public realm. This includes items such as:

- Hot water systems (including solar heating)
- Heating and cooling units
- Water storage tanks
- Satellite dishes and TV aerials
- Solar panels and batteries
- Washing lines.

If service equipment is roof mounted (such as heating and cooling units), these should be positioned below the ridgeline and coloured to match the roof as far as practical.

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Spacious Residential Lots

Lots will accommodate residential development at lower densities to promote generous landscaping and space between buildings. Residential densities range between 1,000 square metres and 2,000 square metres. Allotments are to contain building envelopes (generally measuring 20 metres by 30 metres).

Setbacks

The following setbacks apply:

- | | |
|---------------------|--|
| Front | Dwellings to be setback no less than 6.0 metres from the front street alignment. Porches, verandahs, masonry chimneys, etc. may encroach into the setback distance by no more than 1.0 metre. Eaves, fascias and gutters may encroach into the setback distance by no more than 600mm. |
| Rear | No less than 3.0 metres. |
| Side | No less than 3.0 metres. |
| Garages | Garages should be setback no less than 7.0 metres from the street frontage and should be setback no less than 1.0 metre behind the front building line. |
| Outbuildings | Outbuildings may be permitted to encroach into the rear boundary setback a distance of 50% of the length of the building envelope along the rear boundary. Outbuildings should not be visible from the public realm. |

Building envelope and setback requirements will be registered on title at time of subdivision.

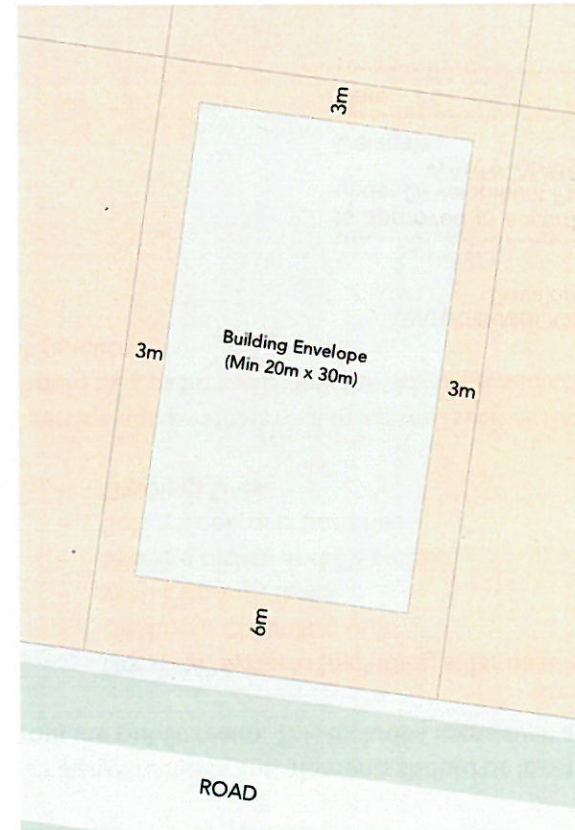


Figure 39: Spacious Residential Lots

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Character

Dwelling facades should include features such as porticos or verandahs to visually break up long walls. Garages should not dominate the appearance of the dwelling and must be a complementary and recessive element to the dwelling facade.

Designs for corner allotments should address both street alignments and/or linking space. Side elevations should be given equal attention to create 'multi-sided' buildings. The design should ensure habitable room windows overlook the side street and/or linking space.

Dwellings within the River Mouth Character Area should be contemporary in design, finish and form and reflect a contemporary coastal style of architecture.

Roof design

Dwellings within the River Mouth Character Area should include a roof design which is simple in style and is integrated with the building design (maximum 14 degree pitch). Roof materials should be matt in finish and non-reflective. Where eaves are to be provided, they are encouraged to be a minimum depth of 450mm.

Materials and colours

Dwellings should incorporate materials which respond to local climatic coastal conditions and should incorporate at least two contrasting materials / finishes to facades to ensure a visually interesting design and enhance streetscape appeal.

External building colours should reflect muted earthy tones which will blend with the coastal setting. A sample of colours for use on building exteriors is provided on pages 93-95.

Fencing

Front fence	Maximum height of 1.5 metres with minimum 50% transparency. Solid fencing below 1.2 metres in height is permitted.
Wing fence	Side wing fences must be setback at least 0.5 metres from the building facade and must contain minimum 50% transparency. Solid fencing is not allowed
Side fences	Fencing (greater than 1.5 metres in height) to a side street alignment should be setback 800mm to provide a landscaping strip along the street alignment. Fencing should have a minimum 50% transparency.

Fencing requirements will be registered on title at time of subdivision.

Access and driveways

Driveways should not dominate front gardens. They should be a secondary element to landscaping and not detract from the appearance of the streetscape.

The design and finish of driveways should complement the design of buildings and landscaping. A sample of materials and finishes is provided on pages 91-93.

Maximum width of crossover must be no wider than 5 metres.

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Garages

Garages and garage doors should complement the design of the dwelling and proposed landscape and be finished with similar or complementary materials.

Garage doors should be of a panel lift door or slim line sectional door variety. Roller doors are not permitted to the facade.

Garage width should not exceed the width of the main dwelling facade.

Landscape

At least three (3) canopy trees should be provided (at least one in the front garden and at least two in the rear garden). Canopy trees must include indigenous species suited to the coastal setting.

Every dwelling must provide for on-site rainwater collection in water tanks for garden irrigation.

Ancillary structures and service equipment

Ancillary structures and elements should be located so that are not readily visible from the public realm. This includes items such as:

- Hot water systems (including solar heating)
- Heating and cooling units
- Water storage tanks
- Satellite dishes and TV aerials
- Solar panels and batteries
- Washing lines.

If service equipment is roof mounted (such as heating and cooling units), these should be positioned below the ridgeline and coloured to match the roof as far as practical.

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Spacious Residential Lots (South West)

Allotments along Hopkins Point Road are designed to ensure dwellings address Hopkins Point Road. Residential densities range between 1,000 square metres and 2,000 square metres.

Setbacks

The following setbacks apply:

- Front** Houses must be designed to have frontage to Hopkins Point Road. Dwellings to be setback no less than 5.0 metres from Hopkins Point Road. Porches, verandahs, masonry chimneys, etc. may encroach into the setback distance by no more than 1.0 metre. Eaves, fascias and gutters may encroach into the setback distance by no more than 600mm.
- Rear** No less than 3.0 metres.
- Side** No less than 3.0 metres.
- Garages** Garages may encroach into the rear setback distance. Garages oriented to the rear road may be built on the building line of this road if the garage doors are at 90 degrees from the road alignment (see precedent images on page 76). A wall on a rear boundary should must be limited to a total length of not more than 7.0 metres on that boundary.
- Outbuildings** Outbuildings should be screened from the public realm.

Building envelope and setback requirements will be registered on title at time of subdivision.

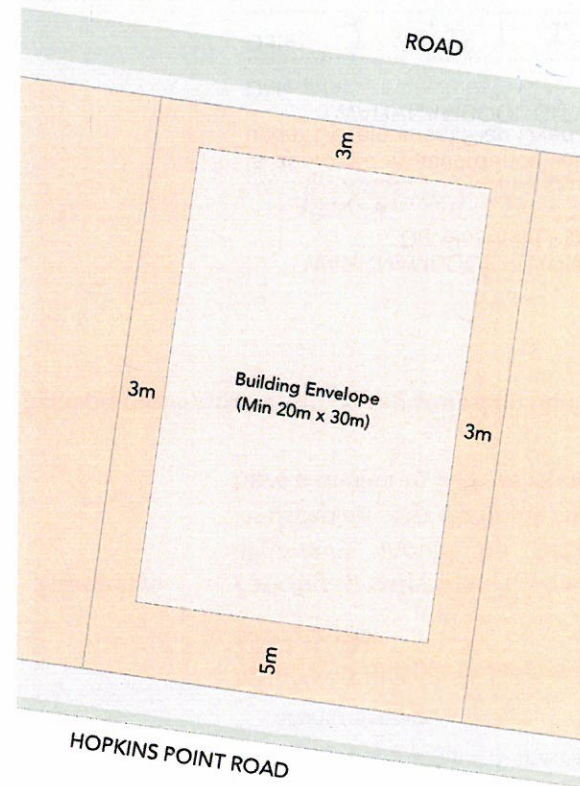


Figure 40: *Spacious Residential Lots (Hopkins Point Road)*

WARRNAMBOOL PLANNING SCHEME
Development Plan for:

Hopkins Heights

Is approved in accordance with the requirements
under Development Plan Overlay - Schedule 13.
WARRNAMBOOL CITY COUNCIL

Delegate:

Date:

2 April 2019

Character

Dwellings should be contemporary in design, finish and form and reflect a contemporary coastal style of architecture.

Dwelling facades should include features such as porticos or verandahs to visually break up long walls. When located within the dwelling facade, garages should not dominate the appearance of the dwelling and must be a complementary and recessive element to the dwelling facade.

Designs for corner allotments should address both street alignments and/or linking space. Side elevations should be given equal attention to create 'multi-sided' buildings. The design should ensure habitable room windows overlook the side street and/or linking space.

Roof design

Dwellings should include a roof design which is simple in style and is integrated with the building design (maximum 14 degree pitch). Roof materials should be matt in finish and non-reflective. Where eaves are to be provided, they are encouraged to be a minimum depth of 450mm.

Materials and colours

Dwellings should incorporate materials which respond to local climatic coastal conditions and should incorporate at least two contrasting materials / finishes to facades to ensure a visually interesting design and enhance streetscape appeal.

External building colours should reflect muted earthy tones which will blend with the coastal setting. A sample of colours for use on building exteriors is provided on pages 93-95.

Fencing

Front fence

Maximum height of 1.5 metres with minimum 50% transparency. Solid fencing below 1.2 metres in height is permitted.

Wing fence

Side wing fences must be setback at least 0.5 metres from the building facade and must be minimum 50% transparency. Solid fencing is not allowed.

Rear fences

Fencing along the rear street alignment should be:

- Setback at least 400mm from the street alignment to provide a landscaping strip, and
- No greater than 1.8 metres in height with minimum 50% transparency.

Planting of hedges or landscaping along street alignments is encouraged.

Side fence

Fencing (greater than 1.5 metres in height) to a side street alignment should be setback 800mm to provide a landscaping strip along the street alignment. Fencing should have a minimum 50% transparency.

Fencing requirements will be registered on title at time of subdivision.

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Access and driveways

Driveways should not dominate front gardens. They should be a secondary element to landscaping and not detract from the appearance of the streetscape.

The design and finish of driveways should complement the design of buildings and landscaping. A sample of materials and finishes is provided on pages 91-93.

Maximum width of crossover must be no wider than 5 metres.

Garages

Garages and garage doors should complement the design of the dwelling and proposed landscape and be finished with similar or complementary materials.

Garage doors should be of a panel lift door or slim line sectional door variety. Roller doors are not permitted to the facade.

Landscape

At least three (3) canopy trees should be provided (at least one in the front garden and at least two in the rear garden). Canopy trees must include indigenous species suited to the coastal setting.

Every dwelling must provide for on-site rainwater collection in water tanks for garden irrigation.

Ancillary structures and service equipment

Ancillary structures and elements should be located so that are not readily visible from the public realm. This includes items such as:

- Hot water systems (including solar heating)
- Heating and cooling units
- Water storage tanks
- Satellite dishes and TV aerials
- Solar panels and batteries
- Washing lines.

If service equipment is roof mounted (such as heating and cooling units), these should be positioned below the ridgeline and coloured to match the roof as far as practical.

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Low Density Bush Lots

Residential densities range between 1,500 square metres and 2,500 square metres and provide space for substantial tree planting around buildings to enhance to vegetation appearance of the development plan area. Allotments are to contain building envelopes (generally measuring 20 metres by 30 metres).

Setbacks

The following setbacks apply:

- | | |
|---------------------|--|
| Front | Dwellings to be setback no less than 6.0 metres from the front street alignment. Porches, verandahs, masonry chimneys, etc. may encroach into the setback distance by no more than 1.0 metre. Eaves, fascias and gutters may encroach into the setback distance by no more than 600mm. |
| Rear | No less than 3.0 metres. |
| Side | No less than 3.0 metres. |
| Garages | Garages should be setback no less than 7.0 metres from the street frontage and should be setback no less than 1.0 metre behind the front building line. |
| Outbuildings | Outbuildings should not be visible from the public realm. |

Maximum building height

Maximum building height of 5.0 metres above natural ground level.

Building envelope, setback and height requirements will be registered on title at time of subdivision.



Figure 41: Low Density Bush Lots

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Date: *2 April 2019*

Character

Dwellings should be contemporary in design, finish and form and reflect a contemporary coastal style of architecture.

Dwelling facades should include features such as porticos or verandahs to visually break up long walls. Garages should not dominate the appearance of the dwelling and must be a complementary and recessive element to the dwelling facade.

Designs for corner allotments should address both street alignments and/or linking space. Side elevations should be given equal attention to create 'multi-sided' buildings. The design should ensure habitable room windows overlook the side street and/or linking space.

Roof design

Dwellings should include a roof design which is simple in style and is integrated with the building design (maximum 14 degree pitch). Roof materials should be matt in finish and non-reflective. Where eaves are to be provided, they are encouraged to be a minimum depth of 450mm. Gable roof forms are not encouraged.

Materials and colours

Dwellings should incorporate materials which respond to local climatic coastal conditions and should incorporate at least two contrasting materials / finishes to facades to ensure a visually interesting design and enhance streetscape appeal.

External building colours should reflect muted earthy tones which will blend with the coastal setting. Dark roof colours are encouraged. A sample of colours for use on building exteriors is provided on pages 93-95.

Fencing

No front fencing shall be permitted (to be managed via a restriction on title).

Side fencing must be constructed 1 metre behind the front facade and return to abut the dwelling.

Fencing requirements will be registered on title at time of subdivision.

Access and driveways

Driveways should not dominate front gardens. They should be a secondary element to landscaping and not detract from the appearance of the streetscape.

The design and finish of driveways should complement the design of buildings and landscaping. A sample of materials and finishes is provided on pages 91-93.

Maximum width of crossover must be no wider than 5 metres.

Garages

Garages and garage doors should complement the design of the dwelling and proposed landscape and be finished with similar or complementary materials.

Garage doors should be of a panel lift door or slim line sectional door variety. Roller doors are not permitted to the facade.

Garage width should not exceed the width of the main dwelling facade.

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Date: *2 April 2019*

Landscape

At least three (3) canopy trees should be provided (at least one in the front garden and at least two in the rear garden). Canopy trees must include indigenous species suited to the coastal setting.

Every dwelling must provide for on-site rainwater collection in water tanks for garden irrigation.

Ancillary structures and service equipment

Ancillary structures and elements should be located so that are not readily visible from the public realm. This includes items such as:

- Hot water systems (including solar heating)
- Heating and cooling units
- Water storage tanks
- Satellite dishes and TV aerials
- Solar panels and batteries
- Washing lines.

If service equipment is roof mounted (such as heating and cooling units), these should be positioned below the ridgeline and coloured to match the roof as far as practical.

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Riverside Lots

In recognition of the precinct's topographic and access constraints, lots within the 'Riverside Lots' area range between 2,000 square metres and 5,000 square metres. Lots have generous setbacks to encourage extensive landscaping.

Setbacks

The following setbacks apply:

- Front** Dwellings to be setback no less than 9.0 metres from the front street alignment. Porches, verandahs, masonry chimneys, etc. may encroach into the setback distance by no more than 1.0 metre. Eaves, fascias and gutters may encroach into the setback distance by no more than 600mm.
- Rear** No less than 25.0 metres (50.0 metres where lots have a shared boundary with the river reserve).
- Side** No less than 5.0 metres.
- Outbuildings** Outbuildings should not be visible from the public realm.

Building envelope and setback requirements will be registered on title at time of subdivision.

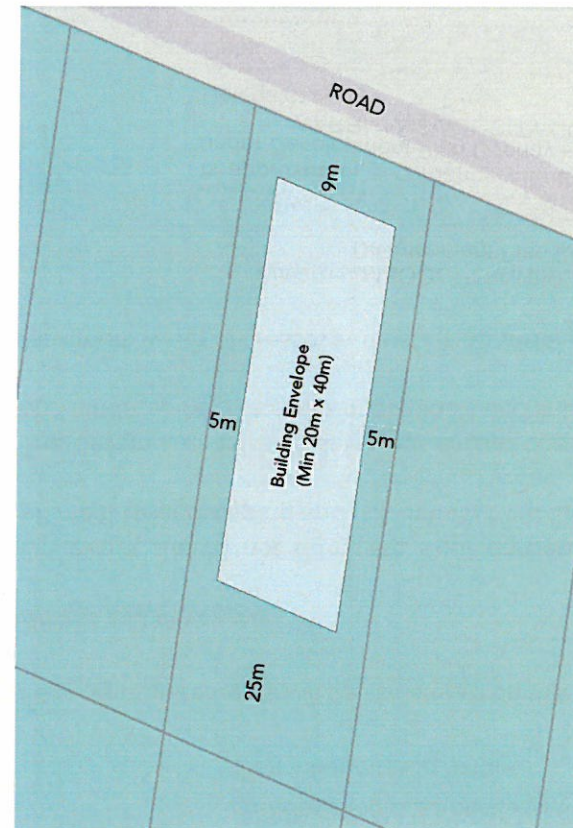


Figure 42: Riverside Lots

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Character

Dwellings should be contemporary in design, finish and form and reflect a contemporary coastal style of architecture.

Dwelling facades should include features such as porticos or verandahs to visually break up long walls. Garages should not dominate the appearance of the dwelling and must be a complementary and recessive element to the dwelling facade.

Designs for corner allotments should address both street alignments and/or linking space. Side elevations should be given equal attention to create 'multi-sided' buildings. The design should ensure habitable room windows overlook the side street and/or linking space.

Roof design

Dwellings should include a roof design which is simple in style and is integrated with the building design (maximum 14 degree pitch). Roof materials should be matt in finish and non-reflective. Where eaves are to be provided, they are encouraged to be a minimum depth of 450mm.

Materials and colours

Dwellings should incorporate materials which respond to local climatic coastal conditions and should incorporate at least two contrasting materials / finishes to facades to ensure a visually interesting design and enhance streetscape appeal.

External building colours should reflect muted earthy tones which will blend with the coastal setting. A sample of colours for use on building exteriors is provided on pages 93-95.

Fencing

No front fencing shall be permitted (to be managed via a restriction on title).

Side fencing must be constructed 1 metre behind the front facade and return to abut the dwelling.

In addition to the above fencing requirements, the following requirements apply to lots which adjoin the biodiversity corridor.

Rear fences

Fencing along the rear boundaries adjoining the biodiversity corridor should be post and wire construction and no greater than 1.5 metres in height.

Fencing requirements will be registered on title at time of subdivision.

Access and driveways

Driveways should not dominate front gardens. They should be a secondary element to landscaping and not detract from the appearance of the streetscape.

The design and finish of driveways should complement the design of buildings and landscaping. A sample of materials and finishes is provided on pages 91-93.

Maximum width of crossover must be no wider than 5 metres.

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Date: *2 April 2019*

Garages

Garages and garage doors should complement the design of the dwelling and proposed landscape and be finished with similar or complementary materials.

Garage doors should be of a panel lift door or slim line sectional door variety. Roller doors are not permitted to the facade.

Garage width should not exceed the width of the main dwelling facade.

Landscape

At least eight (8) canopy trees should be provided (at least three in the front garden and at least five in the rear garden). Canopy trees must include indigenous species suited to the coastal setting.

Every dwelling must provide for on-site rainwater collection in water tanks for garden irrigation.

Ancillary structures and service equipment

Ancillary structures and elements should be located so that are not readily visible from the public realm. This includes items such as:

- Hot water systems (including solar heating)
- Heating and cooling units
- Water storage tanks
- Satellite dishes and TV aerials
- Solar panels and batteries
- Washing lines.

If service equipment is roof mounted (such as heating and cooling units), these should be positioned below the ridgeline and coloured to match the roof as far as practical.

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Hopkins Heights 'village centre'

With vibrant cafes, restaurants and providores within easy walking distances of homes, the Hopkins Heights 'village centre' is the heart of the Hopkins Heights Estate.

The 'village centre' is a central gathering place where people can linger over a late brunch on the weekend or meet clients for lunch meetings, with different menus and venues on offer. On your way to work? Swing by your favourite barista for a takeaway brew and soak in the magnificent vistas which remind you Hopkins Heights is an amazing place to call home.

With providores within walking distance from home, you won't have to go far if you need to dash to the shops for fresh produce. It's the perfect location to visit when you feel like whipping up something special after work.

The 'village centre' will feature well-designed buildings and public spaces which draw people in the make them want to spend time in them. The linear park serves an important role in the connectiveness of the local community, as it provides a range of flexible spaces that cater for different activities. This area will be designed with opportunity for food trucks and temporary market stalls in mind. Most food and drink uses are permissible under the existing General Residential Zone.



Precedent image: street corner café seating example (Tusmo Café Bar, Forde ACT)

Precinct Character

Development (including subdivision, landscaping and construction of dwellings) should respond to the preferred character of each precinct. Design guidelines, including preferred colour palette and materials and finishes for each precinct, is set out on the following pages.

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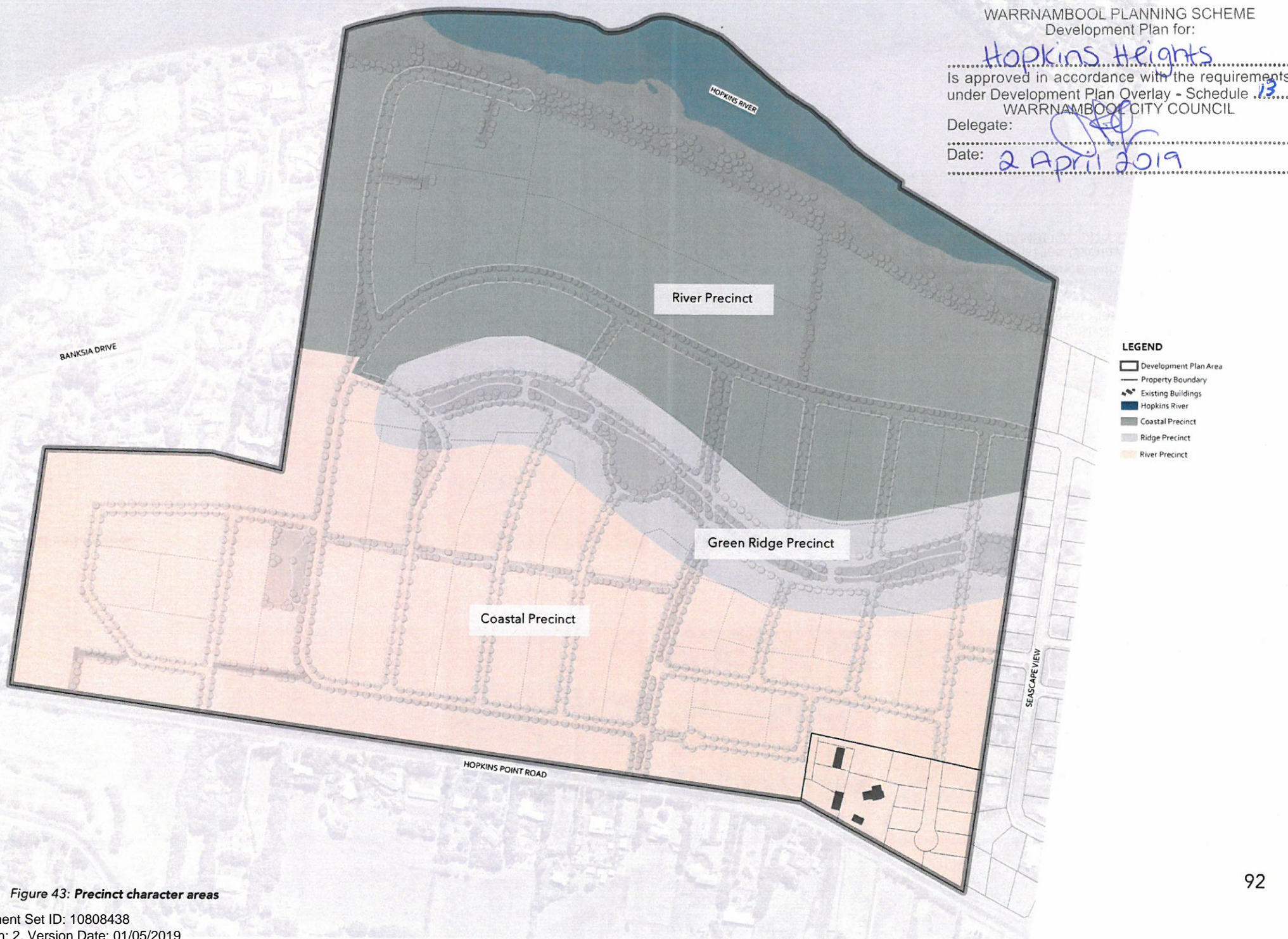
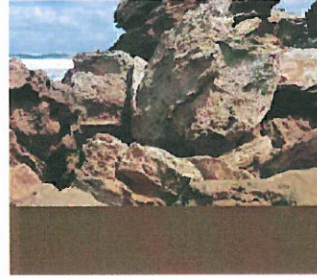
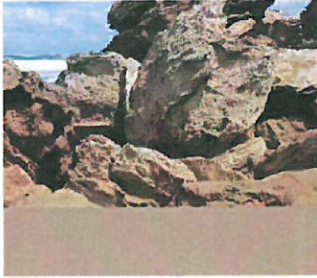


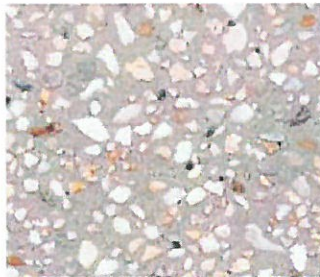
Figure 43: Precinct character areas

Coastal Precinct

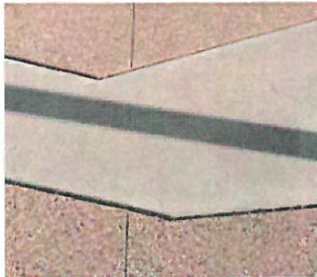
Colour Palette



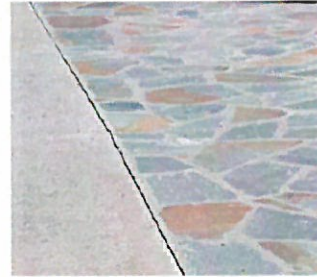
Material and finishes



Exposed aggregate concrete with warm colour finish



Exposed aggregate concrete unit paver with plain concrete paving



Feature paving area

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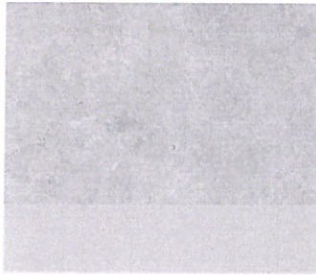
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Green Ridge Precinct

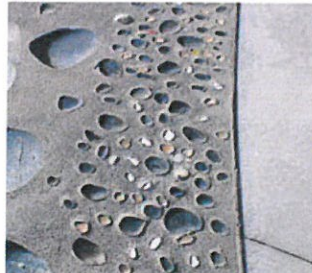
Colour Palette



Material and finishes



Exposed aggregate concrete unit paver



Feature paving area with exposed feature rocks



Granitic sand paving area



Shared path crossing treatment

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River Precinct

Colour Palette



Material and finishes



Exposed aggregate concrete



Granitic sand paving area

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6.4 Utilities and drainage

The development plan area is capable of being serviced by all major urban services.

Water supply

Wannon Water is the responsible authority for the provision of water supply facilities to the development plan area. Reticulated water supply will be provided by the developer at time of construction in accordance with agreement reached with Wannon Water. The development plan makes allowance for a water storage facility (located on the ridge along the eastern boundary of the development plan area) and water pump station located in the north-western corner of the development plan area.

Sewer

Wannon Water is the responsible authority for the provision of sewerage facilities to the development plan area.

All future residential allotments can be serviced via a gravity sewer. A pump station will be located in the north-western corner of the development plan area which will enable future allotments to the north of the ridgeline to connect to the existing sewer. Sewerage infrastructure will be provided by the developer at time of construction in accordance with agreement reached with Wannon Water.

Electricity

Powercor is the responsible authority for the provision of electricity supply to service the development plan area.

Electricity supply can be provided to the area from existing transmission lines. A power kiosk will be required to be installed in order to serve the proposed development¹.

Gas

SP AusNet is the responsible authority for provision of gas reticulation to service the development plan area. A high pressure gas pipeline exists in to the west of the development plan area which is available and the preferred option for connection / extension.

Telecommunications

The National Broadband Network Company (NBNco) is the responsible authority for the provision of telecommunication facilities to service the development plan area. Connection is proposed to be made via an extension to existing services in the area.

¹ To be included as part of the subdivisional works completed during the civil construction phase of the development.

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Drainage

Warrnambool City Council is the responsible authority for stormwater drainage within the development plan area.

A Stormwater Management Plan setting out the existing drainage constraints and future infrastructure is provided with this development plan. In summary, the Stormwater Management Plan identifies stormwater will be collected and treated via proposed infiltration / bio-retention facilities. Post development stormwater flows will be retarded by a series of rain gardens and detention basis, and ultimately discharged into the Hopkins River.

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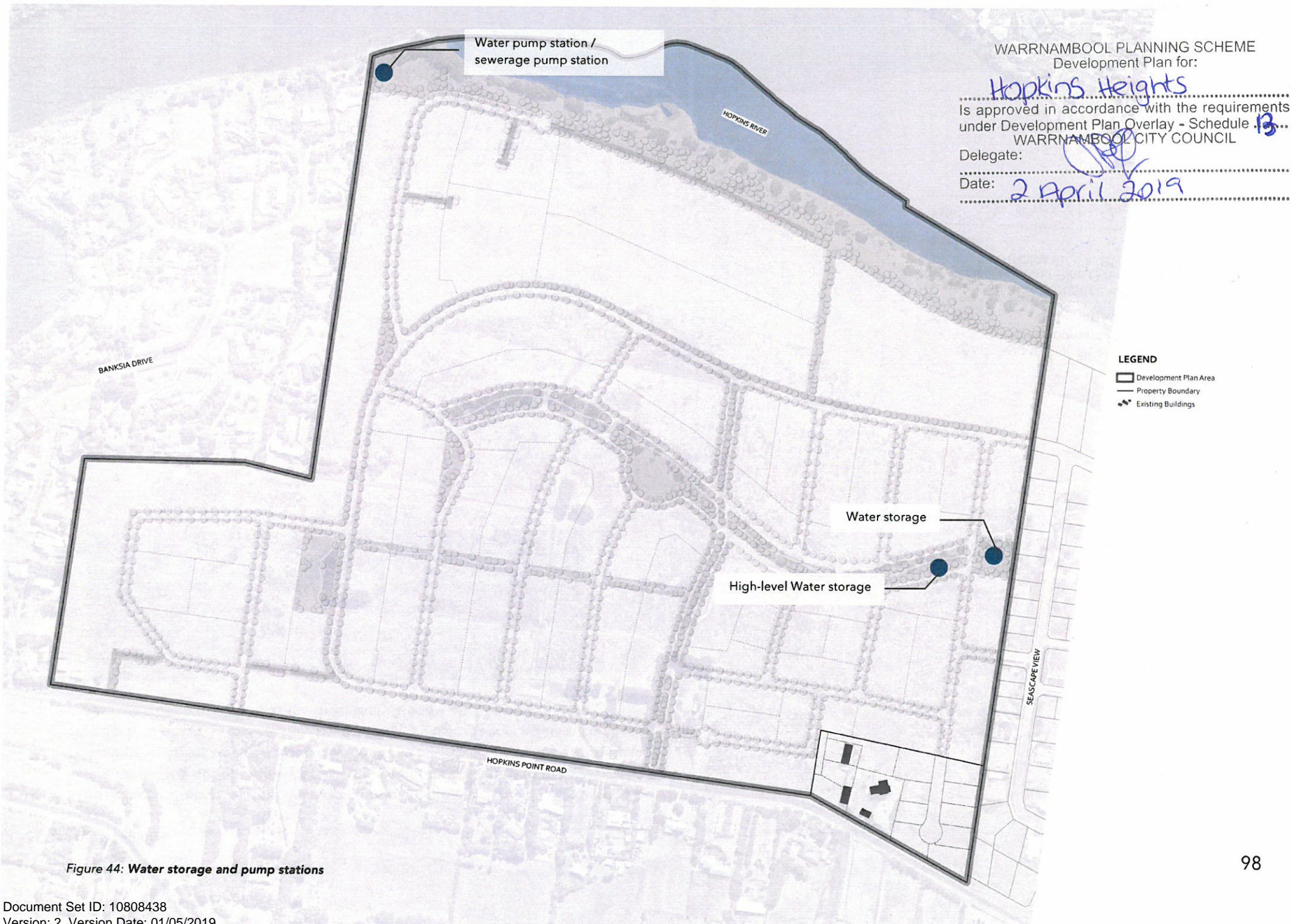


Figure 44: Water storage and pump stations

6.5 Staging

The logical staging of the development plan area will revolve around the incremental roll out of the services as required. Areas located to the east of the development plan area and along Hopkins Point Road will be the first areas which are likely to be developed as they have access to existing services. As such, these areas have been identified as the most likely Stage 1 and 2 areas.

Guidelines for planning permit applications:

- Applications for subdivision must be accompanied with a Servicing Report which sets out likely staging and infrastructure delivery and proposed and ultimate drainage infrastructure (including temporary wetland requirements).

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LEGEND

- Development Plan Area
- Property Boundary
- Existing Buildings
- Hopkins River
- Indicative development staging direction

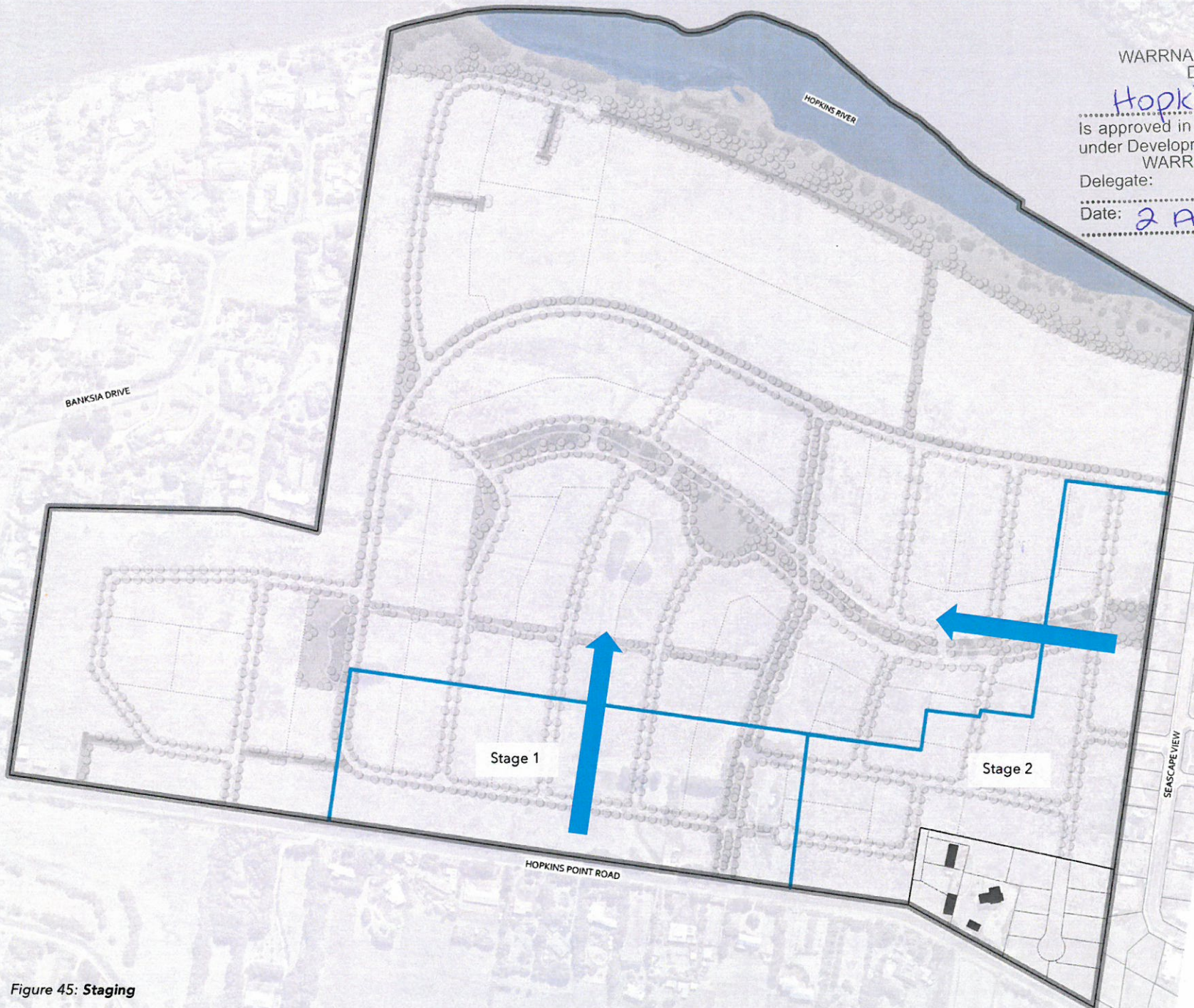


Figure 45: Staging

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